

PART II – City of Marshalltown ADA Self-Evaluation & Transition Plan for Street Rights-of-Way

I. SELF-EVALUATION

A. Overview

The City of Marshalltown is required, under Title II of the Americans with Disabilities Act (ADA) and 28 C.F.R Sec. 35.105, to perform a Self-Evaluation of current transportation infrastructure policies, practices, and programs. A Self-Evaluation identifies what policies and practices impact accessibility and examine how the City implements these policies. The goal of the Self-Evaluation is to verify that, in implementing the City's policies and practices, the department is providing accessibility and not adversely affecting the full participation of individuals with disabilities.

The Self-Evaluation also examines the condition of the City's Pedestrian Circulation Route/Pedestrian Access Route (PCR/PAR) and identifies potential need for PCR/PAR infrastructure improvements. This will include the sidewalks, curb ramps, bicycle/pedestrian trails, traffic control signals, and transit facilities that are located within the City public rights-of-way. Any barriers to accessibility identified in the Self-Evaluation and actions to remedy identified barriers are identified in this document. Pedestrian facilities were evaluated for ADA deficiencies throughout the City.

B. Methodology

The Self-Evaluation field data inventory began in mid-May 2019 and concluded in October 2019. During this time, project staff inventoried components of the PCR/PAR environment using the latest GPS technology to collect field data for pedestrian infrastructure features. Data was imported into Esri ArcGIS for analysis, reporting, and mapping as part of the Transition Plan. ADA accessibility evaluation included the following:

- *Pedestrian Ramps* – All pedestrian ramps were inventoried and evaluated for accessibility. Ramps were assigned a level of accessibility based on the presence of a ramp, presence of detectable warnings such as truncated domes, presence of a landing, and maintenance issues. Maintenance issues include vertical discontinuity, gaps, cracking, standing water, vegetation, spalling, and others as shown in **Appendix A** of this document.

Ramps received one of the following accessibility ratings based on the above criteria:

1: Generally Accessible. All ADA elements are present and there are no maintenance issues.

2: Mostly Accessible. Some ADA elements are missing or maintenance issues exist.

3: Mostly Inaccessible. Most ADA elements are missing and/or maintenance issues exist.

4: Not Accessible. Ramp is not present or all ADA elements are missing and maintenance issues exist.

An accessible pedestrian ramp must achieve a condition rating of 1 or 2. Pedestrian ramps receiving a condition rating of 3 or 4 are not considered accessible and are candidates for future reconstruction projects. The timeline for modification of each of these pedestrian ramps will depend on its proximity to priority locations, correlation to planned projects, reasonable accommodation requests, and available funding. Pedestrian ramp replacement is anticipated to cost approximately \$4,000 per ramp which includes design and construction of one ramp. Pedestrian ramp accessibility is identified in **Appendix B**.

- **Sidewalks and Trails** – Sidewalk and trail maintenance issues include vertical discontinuity, gaps, cracking, standing water, vegetation, spalling, among others. Barriers to accessibility include items obstructing the PAR which could include hydrants, lighting/traffic signal poles, power poles, manhole/handhole, gate valves, and locations with a narrowed PCR/PAR among others (**Appendix A**).

Sidewalks and trails received one of the following condition ratings based the above criteria:

- 1:** Sidewalk is smooth with no vertical discontinuities
- 2:** Sidewalk has vertical discontinuities less than 1/2 inch, and the surface is still passable
- 3:** Sidewalk has vertical discontinuities more than 1/2 inch
- 4:** Sidewalk is crumbling, has many cracks, and is unpassable for wheelchairs in many spots

An accessible sidewalk or trail must achieve a condition rating of 1 or 2. Sidewalk and trail segments exhibiting condition ratings of 3 or 4 are not accessible and are high priority candidates for future reconstruction projects.

- **Crosswalks** – Crosswalks were evaluated for their general condition. Marked crosswalk locations were assessed for marking visibility issues and general pavement condition.
- **Traffic Signals** – Available pedestrian signals were inventoried for APS availability. Traffic signals were categorized as APS and Non-APS locations. Signals without APS are not accessible.
- **Transit Stops and Shelters** – Transit stops and shelters were inventoried for location and accessibility. Inventory included connection to PAR, shelter accessibility (if present), and presence of a level landing area.
- **Parking Lots** – Parking lots were inventoried and evaluated for accessibility. Information collected included number of spaces, number of accessible spaces, van accessibility, signage, markings, route connecting to PCR/PAR,

and site route/connections. The conditions of each parking lot are provided in **Table B-2** and **Figure B-23** in **Appendix B**.

- **Parks** – A field investigation of public parks was conducted to evaluate accessibility. Information collected included sidewalk/trail accessibility and ADA compliance of shelters and restrooms.
- **Facilities** – Facilities including public buildings and spaces were also evaluated as part of this effort. This part of the evaluation is detailed in **Part II** of this document. Information collected as part of the facilities evaluation includes interior/exterior accessible routes, and accessibility features of parking, entryways, building interiors, ramps, elevators, signs, doors, controls, seating, service counters, restrooms, drinking fountains, and fire alarms.

C. Summary

The City of Marshalltown conducted the Self-Evaluation review of programs from May-September 2019. Results of the Self-Evaluation are included in the **Figure 1**. **Appendix B** provides more information on the results of the Self-Evaluation including an implementation plan for making the City’s facilities accessible.

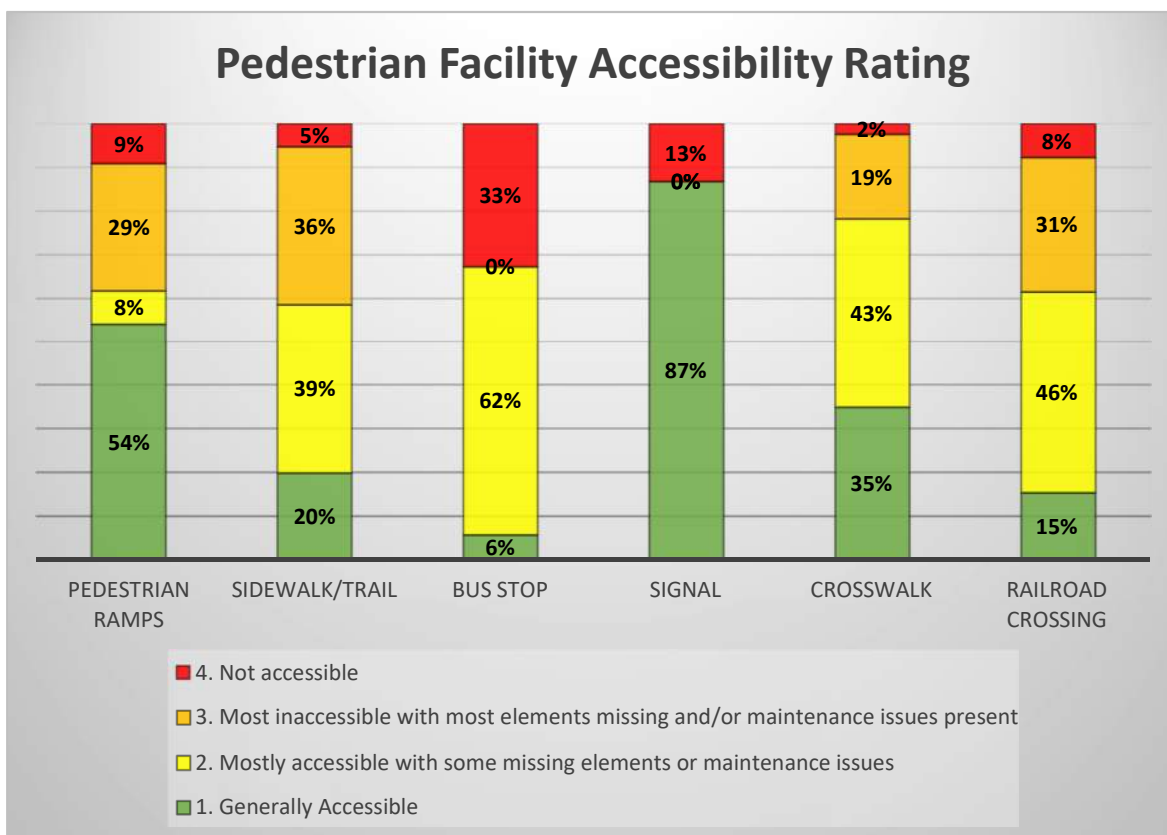


Figure 1. Self-Evaluation Results with Rated Levels of Accessibility

D. Additional Recommendations

There are numerous locations throughout the City where sidewalk terminates well short of the street intersection, often at a driveway or in the boulevard with no ramp access. This was considered more of a gap in the system rather than a “No

Ramp” situation during the evaluation, which goes beyond the scope of this accessibility review. Nevertheless, this scenario poses limitations to system accessibility. It’s a recommendation that the City perform a sidewalk gap analysis to identify these locations and other similar gaps and create a plan for removing these barriers and completing existing infrastructure.

II. POLICIES AND PRACTICES

The City of Marshalltown is required, under Title II of the ADA and 28 CFR 35.105, to perform a self-evaluation of its policies, practices, and programs. The goal of this Self-evaluation is to verify that, in implementing the policies and practices, the City is providing accessibility and not adversely affecting the full participation of individuals with disabilities. The Self-evaluation identifies policies and practices that affect accessibility and examine City implementation of these policies. The Self-evaluation examines the condition of the City's PCR/PARs and identifies any existing infrastructure needs.

Appendix B illustrates City pedestrian infrastructure.

A. Previous Practices

Since the adoption of the ADA, the City of Marshalltown has striven to provide accessible pedestrian features as part of the City's capital improvement projects. As additional information was made available as to the methods of providing accessible pedestrian features, the City updated its procedures to accommodate these methods. The City also applied for and received grants to expand and improve existing pedestrian facilities in compliance with ADA.

B. Policy

The City of Marshalltown's goal is to continue to provide accessible pedestrian design features as part of the City’s capital improvement projects. The City has established ADA design standards and procedures as listed in **Appendix G**. These standards and procedures will be kept up to date with nationwide and local best management practices.

The City will consider and respond to all accessibility improvement requests. All accessibility improvements that have been deemed reasonable will be scheduled consistent with transportation priorities. The City will coordinate with external agencies to ensure that all new or altered pedestrian facilities within the City’s jurisdiction are ADA compliant to the maximum extent feasible.

Requests for accessibility improvements can be submitted to the City of Marshalltown’s ADA Coordinator. Contact information for ADA Coordinator is located in **Appendix F**.

Maintenance of pedestrian facilities within the public rights-of-way will continue to follow the policies set forth by the city. A breakdown of relevant policies can be seen in **Appendix B** which includes the following:

- City of Marshalltown Comprehensive Plan (2012)
- IA Highway 14 Corridor Study (2018)

- Standard Specifications for Portland Cement Concrete Sidewalks and Driveways (2005)
- City of Marshalltown City Code

New Construction

All new street construction projects with pedestrian accommodations will be designed and constructed to conform with the most current ADA guidance and design practices to the maximum extent feasible.

Reconstruction Projects

All city street reconstruction projects with pedestrian accommodations will be designed and constructed in accordance with the most current ADA guidance and design practices to the maximum extent feasible.

Rehabilitation/Resurfacing projects

All city rehabilitation and resurfacing projects will include accessible pedestrian curb ramps as needed to provide access to existing pedestrian facilities (i.e. walks/trails) at locations where they do not currently exist. Improvements to existing pedestrian ramps will be addressed on a case by case basis. High priority areas such as those in close proximity to specific land uses (i.e. schools, government offices, and medical facilities) will be given additional consideration. Improvements will be undertaken at the discretion of the City Engineer.

Stand Alone Projects

As grant funding opportunities are available, independent ADA projects may be undertaken by the City. A brief engineering study will be performed where candidate sites will be evaluated on facility condition, pedestrian volumes, public safety, public benefit and improvement costs as well as the ability to provide alternative barrier removal options.

Development Projects

As private development continues to grow, both new and reconstruction, the City of Marshalltown will require new and adjacent infrastructure to comply with ADA standards.

III. IMPLEMENTATION PLAN

A. Priority Areas

All intersections and roadway segments in the City of Marshalltown were classified based on the following criteria:

Priority Level 1 – High Priority

- Locations exhibiting accessibility barriers identified through the public process by stakeholders and the general public.
- Locations and roadway segments serving Level 1 facilities including:

- Government facilities (city, city, state);
- Public libraries;
- Public and private primary and secondary schools (within a one-quarter mile radius from the school property);
- Hospitals, health clinics and health centers (public and private);
- Public housing and homeless shelters, including senior facilities and rehabilitation facilities;
- Colleges, universities, and technical schools;
- Transportation hubs (includes bus lines and transit stations);
- Parks

For these high priority locations and roadway segments, field collection staff measured a variety of detailed accessibility and pedestrian data, as described in **Section III**.

Priority Level 2 – Medium Priority

- Locations and roadway segments serving Level 2 facilities including:
 - Central business districts, shopping malls, supermarkets and strip retail centers;
 - Churches and Places of Worship
 - Major employment sites;
 - Housing complexes, including apartments

For these medium priority locations and roadway segments, field collection staff measured a detailed variety of accessibility and pedestrian data.

Priority Level 3 – Lower Priority

- Single-family residential areas;
- Industrial areas;
- Other areas not classified as Priority Level 1 and 2

The City of Marshalltown used the priority ranking outlined above to create the plan and schedule for integrating ADA compliance projects in future street projects. Reporting on the results of the Self-Evaluation is included in **Appendix B**. The City will work with each community in its jurisdiction to implement improvements identified in this ADA Transition Plan.

B. External Agency Coordination

Many other agencies are responsible for pedestrian facilities within the jurisdiction of the City of Marshalltown. The City will coordinate with those agencies to track and assist in the facilitation of the elimination of accessibility barriers along their routes.

C. Schedule

Overall, this Plan estimates \$11.1 M in ADA improvements are necessary for pedestrian facilities throughout the City. The City aims to achieve ADA compliance among these

facilities responsibly, and as expeditiously as possible, recognizing that \$11.1 M is a significant cost.

The City will utilize three methods for upgrading pedestrian facilities to the current ADA standards. The first and most comprehensive of the three methods are scheduled street and utility improvement projects. All pedestrian facilities impacted by these projects will be upgraded to current ADA accessibility standards. The second method is the stand alone sidewalk and ADA accessibility improvement project. These projects will be incorporated into the Capital Improvement Program (CIP) on a case by case basis as determined by City staff. Lastly, the City will use site redevelopment, new development, and cooperative agreement projects to bring all adjacent pedestrian facilities into compliance.

New instances of non-compliance will surface each year as normal wear-and-tear and extreme weather take their toll on pedestrian facilities. These conditions will likely prevent any Iowa City from achieving 100% ADA compliance among facilities. However, deficiencies have been carefully prioritized in this plan to ensure steps are taken where they matter most, and in the shortest timeframe possible. With this in mind, the City aims to improve deficiencies identified in this plan over a 40-year period, allocating on average \$280,000 annually through the project types identified above.

The City has set the following implementation goals for improving the accessibility of its pedestrian facilities within its jurisdiction:

- After 10 years, 25% of accessibility features identified in this Plan would be improved.
- After 20 years, 50% of accessibility features identified in this Plan would be improved.
- After 30 years, 75% of accessibility features identified in this Plan would be improved.
- After 40 years, 100% of accessibility features identified in this Plan would be improved.

The City anticipates updating this plan every five years to capture new instances of non-compliance found and to document progress achieved since the previous plan was adopted. This plan serves as a framework for the City to follow and implementation goals will likely change as the City pursues ADA compliance.

D. Budget Information – Planning Level Costs

Construction costs for upgrading facilities can vary depending on each individual improvement and conditions of each site. Costs can also vary on the type and size of project the improvements are associated with. Listed below are representative 2018 costs for some typical accessibility improvements. These costs were used to generate planning level cost estimates for ADA improvement projects.

Unit Prices		
Description	Unit	Per Unit Cost
Pedestrian Ramp Construction	Ramp	\$4,000
Traffic Control Signal APS Upgrade Retrofit	APS Equipment	\$15,000
Sidewalk/Trail ADA Improvement Retrofit	Sq. Ft.	\$5.00
Bus Stop/Shelter ADA Improvement Retrofit	Bus Stop	\$4,000

E. Undue Financial Burden

Some ADA improvement projects can pose financial burdens on communities that are not justified. Consider the following example:

A portion of a sidewalk includes a driveway apron with a running slope exceeding 2% grade. The driveway apron running slope serves as the cross slope of the intersecting sidewalk which has a cross slope tolerance of <2% grade to maintain ADA compliance. The only option for achieving compliance is to purchase right-of-way from the adjacent property owner to remedy the issue.

Obtaining right-of-way is often controversial among property owners and can be cost prohibitive. While one or two instances of this may not pose a considerable financial burden, the burden is compounded when multiple instances occur and the costs add up. The City of Marshalltown plans to meet this challenge by ensuring future pedestrian infrastructure is constructed according to ADA standards from the start, ensuring the safety and accessibility of pedestrian infrastructure and avoiding future potential for undue financial burden.

Given the magnitude of non-compliance in the community, the City of Marshalltown feels the scheduling and prioritization set forth in this plan provides the most equitable, effective use of the agency's already constrained financial resources for bringing infrastructure into compliance. This includes performing what would be stand-alone ADA improvement projects in larger, programmed projects in the City's CIP or STIP projects beginning with areas of high priority. Infrastructure deficiencies have been carefully prioritized in this process based on areas of high pedestrian traffic, input from the public, and condition of facilities. If instances of non-compliance pose a major threat to those using pedestrian facilities, the City of Marshalltown will remedy deficiencies immediately upon becoming aware of the issue. All decisions surrounding priority will be data-driven, defensible, and will ensure an objective process that is documented in writing has been performed as the basis for those decisions.

Under the 23 CFR 35.150(a)(3), if a public agency believes and can demonstrate that a requested action to make a facility accessible would result in a fundamental alteration in the nature of a service, program, or activity or in undue financial burden, *"...a public agency has the burden of proving that compliance with §35.150(a) would result in such alteration or burdens. The decision that compliance would result in such alteration or burdens must be made by the head of a public entity or his or her designee after considering all resources available for use in the funding and operation of the service, program, or activity, and must be accompanied by a written statement of the reasons for reaching that conclusion..."* (www.ecfr.gov).

IV. ADA COORDINATOR

In accordance with 28 CFR 35.107(a), the City of Marshalltown has identified an ADA Title II Coordinator to oversee the City's policies and procedures. Contact information for this individual is in **Appendix F**.

V. PUBLIC OUTREACH

The City of Marshalltown recognizes that public participation is an important component in the development of this document. Input from the community has been gathered and used to help define priority areas for improvements within the public rights-of-way of the City of Marshalltown.

Public outreach for the creation of this document consisted of the following activities:

- Grievance Procedure:

In compliance with 28 CFR 35.107, the City has established the grievance procedure to take future comments and concerns for the City. This can be seen in **Appendix E** and is further detailed in Section VIII below.

- City Website:

A City website was established early in the process that provided the public an opportunity to submit comments on areas in the community they find problematic.

This document was available for public comment online between the dates of January 20, 2020 and February 17, 2020. Information regarding the public outreach activities is in **Appendix D**.

- Public Open House:

A public open house event was held August 22, 2019 at the City of Marshalltown Public Library to solicit feedback from the public on the draft ADA Transition Plan and projects in the City that pose barriers to accessibility that weren't identified in the plan. Feedback from this meeting informed the prioritization of projects in the implementation plan. A meeting summary can be seen in **Appendix D**.

VI. GRIEVANCE PROCEDURE

Under the Americans with Disabilities Act, each agency is required to publish its responsibilities in regard to the ADA. A draft of this public notice is provided in **Appendix E**. If users of the City of Marshalltown facilities and services believe the City has not provided reasonable accommodation, they have the right to file a grievance.

In accordance with 28 CFR 35.107(b), the City has developed a grievance procedure for the prompt and equitable resolution of citizens' complaints, concerns, comments, and other grievances. This grievance procedure is outlined in **Appendix E**.

VII. MONITOR THE PROGRESS

This document will be updated as needed to reflect the City of Marshalltown's approach to complying with ADA and providing accessible pedestrian infrastructure. The appendices in this document will be updated periodically to account for improvements, while the main body of the document will be updated within five to ten years with a future update schedule to be developed at that time. With each main body update, a public comment period will be established to continue the public outreach.

Appendix A: Deficiencies

I. Appendix A: Self-Evaluation – Examples of Commonly Identified Deficiencies and Obstructions

Figure A.1 – Commonly Identified Deficiencies



Vertical Discontinuity



Horizontal Discontinuity



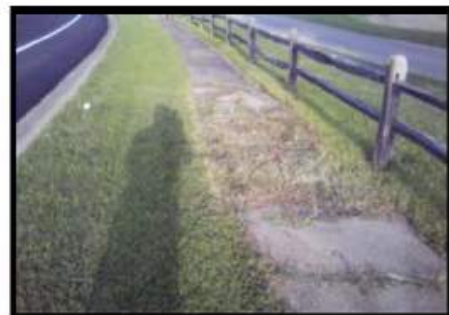
Cross Slope



Cracking



Ponding



Vegetation



Spalling



Vertical Slope

Figure A.2 – Sidewalk Obstruction Examples



Fire Hydrant



Lighting Pole



Traffic Signal Handhole



Traffic Signal Pole



Utility Pole



Water Gate Valve

Appendix B: Self Evaluation

I. Appendix B: City of Marshalltown, IA Self-Evaluation

A. Review of Policies and Practices

Policy Recommendations

The City's policies and practices include any City, department, or division policies and practices that direct staff in its daily work activities. Policies and practices that relate to accessibility and ADA conformance include:

1. *Marshalltown Comprehensive Plan (2012)*

a) Chapter 4 – Transportation

This chapter provides immediate goals and policies for promoting and expanding opportunities for safe pedestrian and bicycle traffic. It also calls for the integration of pedestrian facilities into the design of roadway facilities throughout the community.

Chapter 5 - Parks, Trails & Open Space

This chapter summarizes the importance of the health and well-being of the community through active transportation and activities in the city parks and trails. It calls for integration of pedestrian facilities in all land use areas of the city and to replace and maintain pedestrian facilities regularly.

Policy Understanding and Recommended Revision:

The following revision to this policy is recommended as part of this review:

1. Amend the Plan to include policies for the compliant design of pedestrian facilities based on best practices outlined in the Public Rights-of Way Accessibility Guidelines (PROWAG) as adopted by IDOT.

2. *IA Highway 14 Corridor Study (2018)*

This plan addresses pedestrian issues along Highway 14 including constrained ROW that create narrow sidewalks and limited pedestrian facilities. It also summarizes concerns from the public on constrained pedestrian facilities and non-compliant facilities.

Study Understanding and Recommended Action:

The following is recommended when implementing study recommendations along the Highway 14 Corridor:

1. Ensure the compliant design of pedestrian facilities based on best practices outlined in the Public Rights-of Way Accessibility Guidelines (PROWAG) as adopted by IDOT.

3. *Standard Specifications for Portland Cement Concrete Sidewalks and Driveways (2005)*

This plan addresses construction guidelines and design requirements for sidewalks in the city. It acknowledges that detectable warning surfaces are an ADA requirement and required when constructing or rehabbing curb ramps and street surfaces.

Policy Understanding and Recommended Revision:

The following revision to this policy is recommended as part of this review:

2. Amend the Standard Specifications to include policies for the compliant design of pedestrian facilities based on best practices outlined in the Public Rights-of Way Accessibility Guidelines (PROWAG) as adopted by IDOT.

4. *The City of Marshalltown City Code*

The City of Marshalltown City Code provides several provisions regarding sidewalk infrastructure including maintenance, responsibilities of property owners and tenants, nuisances to sidewalks, and design specifications for the construction and placement of sidewalks.

Policy Understanding and Recommended Revision:

While the City Code provides actions for sidewalk installation and maintenance, it does not address adherence to ADA compliance or reference to federal/state ADA design standards or guidelines.

The following revision to this policy is recommended as part of this review:

3. Amend City Code Article III to include adherence to ADA standards and guidance as outlined in the Public Rights-of Way Accessibility Guidelines (PROWAG) and IDOT for reconstruction.

B. Review of Programs

The City of Marshalltown inventoried pedestrian ramps and sidewalks within the public rights-of-way and public parking lots beginning in November of 2017 and ending in September 2018. The following provides the results of that inventory:

1. Pedestrian Ramps

The City of Marshalltown has 2663 locations where pedestrian ramps exist or should exist within its public rights-of-way and public parking lots combined. These locations were inventoried for ADA accessibility based on (1) whether a pedestrian ramp was present and (2) if accessibility criteria outlined in **Part 1 Sec. II** were met. 1644 (62%) of the locations were reported to be generally accessible or mostly accessible. 1019 (38%) were reported to be non-compliant.

2. Sidewalks/Trails

The sidewalks and trails within the City of Marshalltown public rights-of-way and public parking lots were inventoried and evaluated to determine existing accessibility and to identify existing obstructions based on accessibility criteria outlined in **Part 1**. Of the 137 miles of sidewalk/trail evaluated, 56.7 miles (41%) had a condition rating of three or four meaning they were mostly inaccessible and require further evaluation and remedial action.

Crosswalks

Crosswalks were inventoried and evaluated for the quality of markings and pavement condition. Of 1038 crosswalks evaluated, 226 were identified as inaccessible.

3. Traffic Signals

53 traffic signals were evaluated for the presence of Accessible Pedestrian Signal (APS) technology. Each pedestrian crossing push button location was inventoried as a separate location and identified as non-compliant if APS technology was not present. Seven locations were without APS technology.

4. Bus Stops

There are 52 total bus stops in the City of Marshalltown. 17 (33%) of the stops were identified as inaccessible

5. Railroad Crossings

There are 13 locations where pedestrian facilities cross railroad tracks. 5 (39%) were identified as inaccessible.

The following figures and tables report the status of accessible and inaccessible pedestrian infrastructure in the public right-of-way as well as public parking lots:

- **Figures B-1 through B-22** illustrate all infrastructure inventoried
- **Table B-1** provides an implementation plan for priority ranked improvements. The timeline for replacement of these sidewalks is dependent on priority ranking, correlation to planned projects, reasonable accommodation requests, and available funding.
- **Figure B-23** illustrates parking lot locations
- **Table B-2** provides a breakdown of the public parking lot evaluation.

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Legend



Pedestrian Ramp

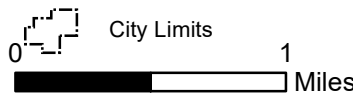
- 1. Generally Accessible (1,441 - 54%)
- 2. Mostly accessible with some missing elements or maintenance issues (203 - 8%)
- 3. Most inaccessible with most elements missing and/or maintenance issues present (777 - 29%)
- 4. Not accessible (242 - 9%)

Sidewalk

- 1. Generally Accessible (27.3 mi - 20%)
- 2. Mostly accessible with some missing elements or maintenance issues (53 mi - 39%)
- 3. Most inaccessible with most elements missing and/or maintenance issues present (49.4 mi - 36%)
- 4. Not accessible (7.2 mi - 5%)

Bus Stop

- 1. Generally Accessible (3 - 6%)
- 2. Mostly accessible with some missing elements or maintenance issues (32 - 61%)
- 3. Most inaccessible with most elements missing and/or maintenance issues present (0 - 0%)
- 4. Not accessible (17 - 33%)
- Stakeholder Comments



Source: IADOT, City of Marshalltown, Marshall County

Signal

- Accessible Pedestrian Signal Present (46 - 88%)
- Accessible Pedestrian Signal Not Present (6 - 12%)

Crosswalk

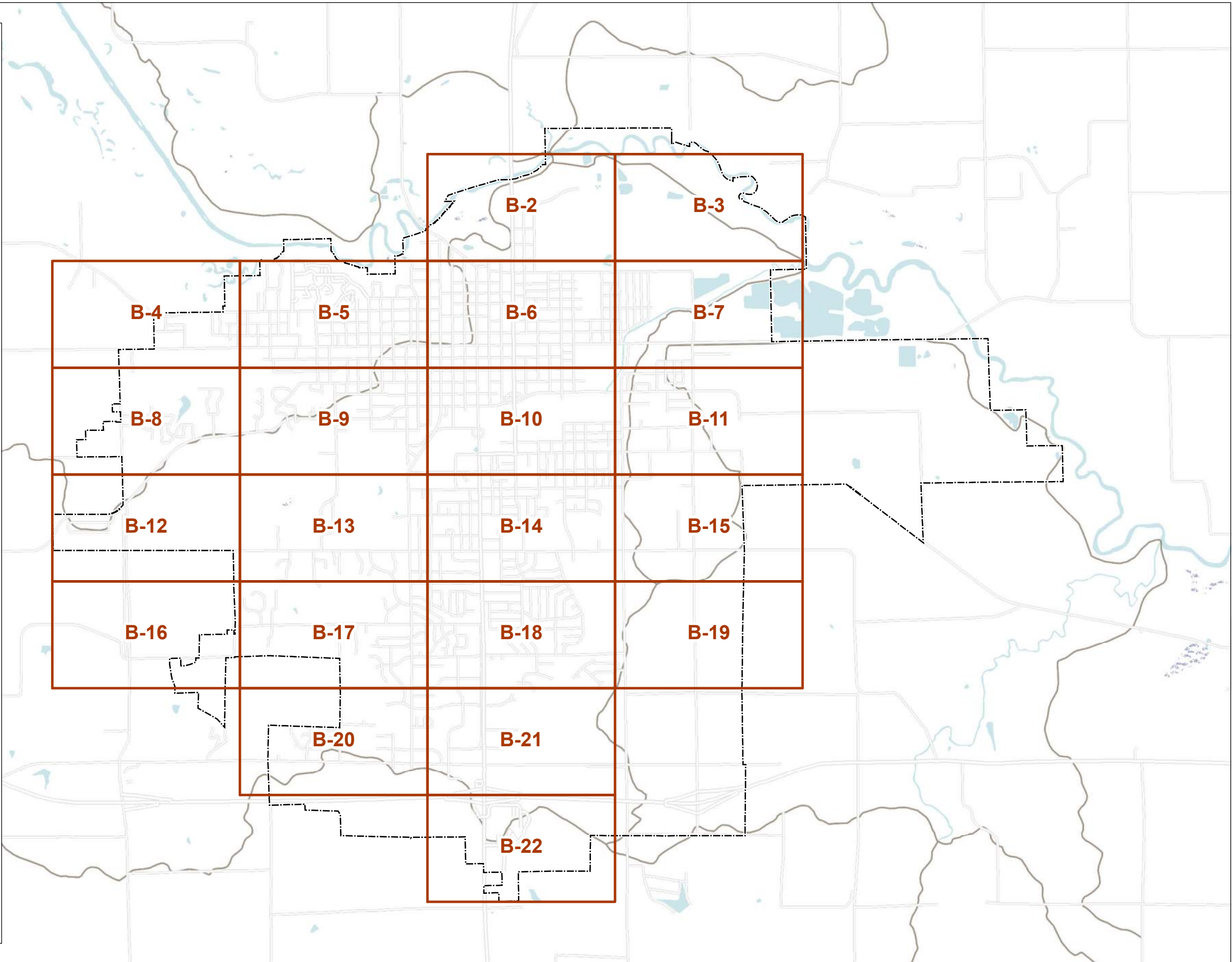
- 1. Generally Accessible (362 - 35%)
- 2. Mostly accessible with some missing elements or maintenance issues (450 - 43%)
- 3. Most inaccessible with most elements missing and/or maintenance issues present (201 - 19%)
- 4. Not accessible (25 - 2%)

Railroad Crossing

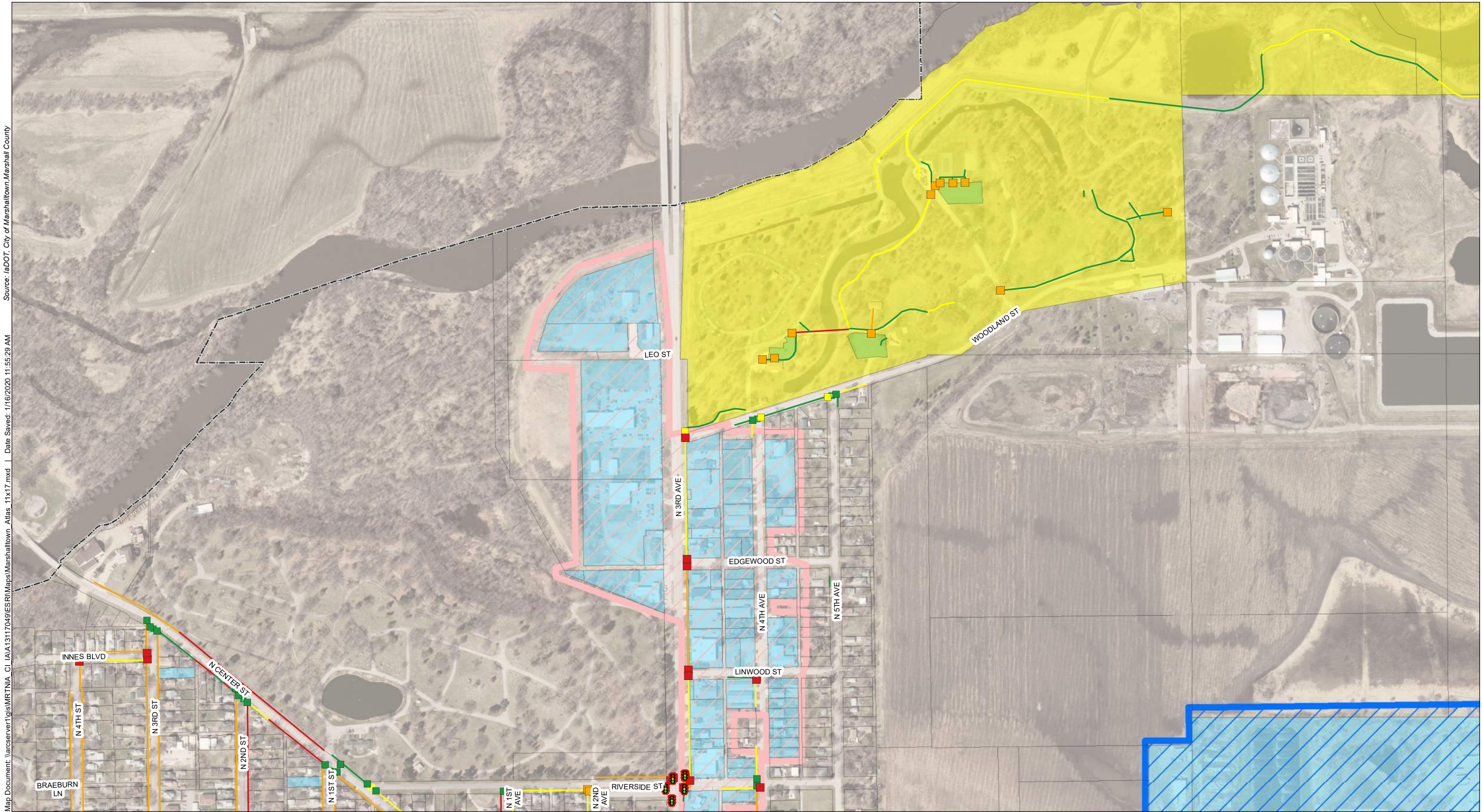
- 1. Generally Accessible (2 - 15%)
- 2. Mostly accessible with some missing elements or maintenance issues (6 - 46%)
- 3. Most inaccessible with most elements missing and/or maintenance issues present (4 - 31%)
- 4. Not accessible (1 - 8%)

- City Parking Lot
- Priority 1 Area
- Priority 2 Area
- Central Business District
- Commercial District
- Major Employment District

Figure B-1

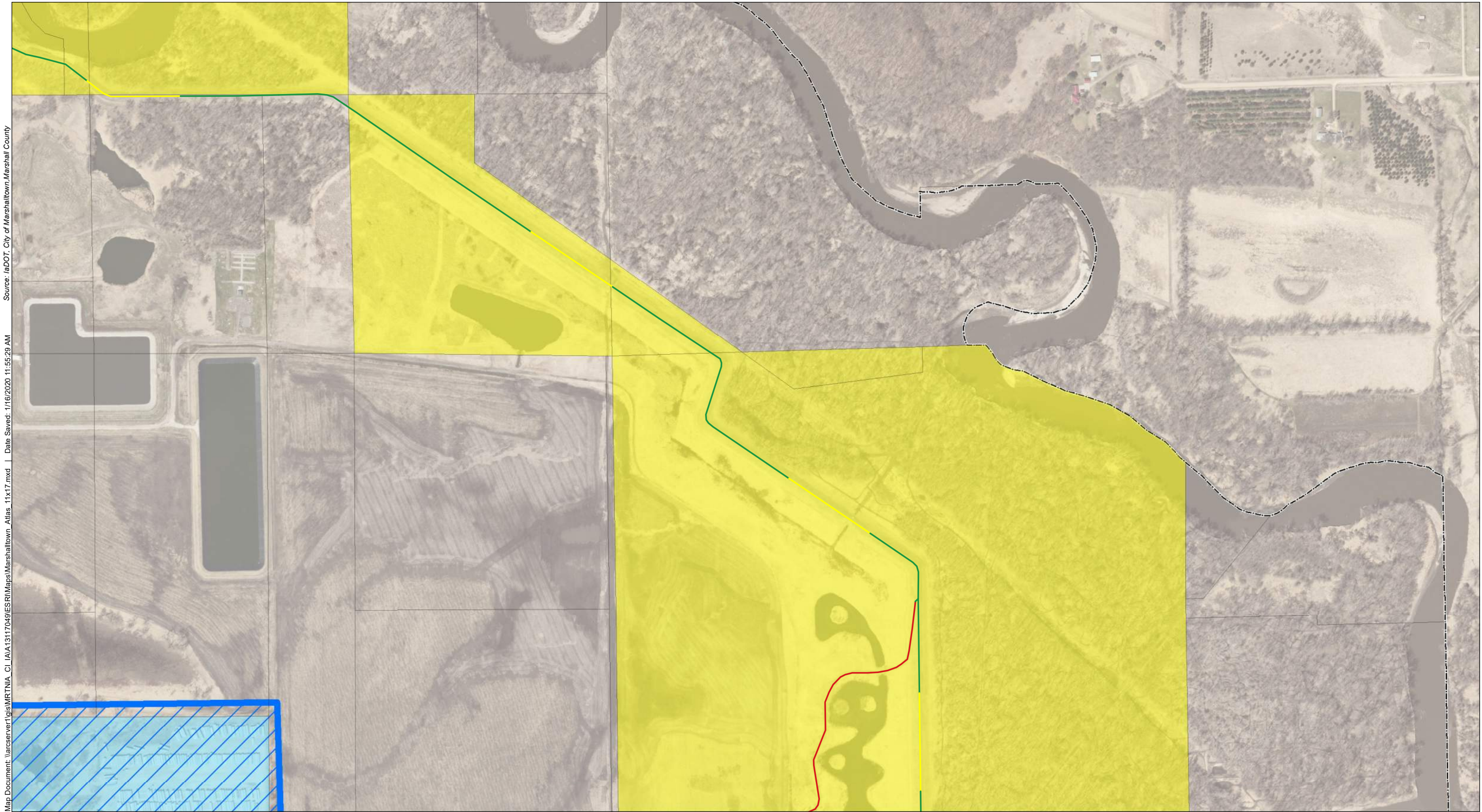


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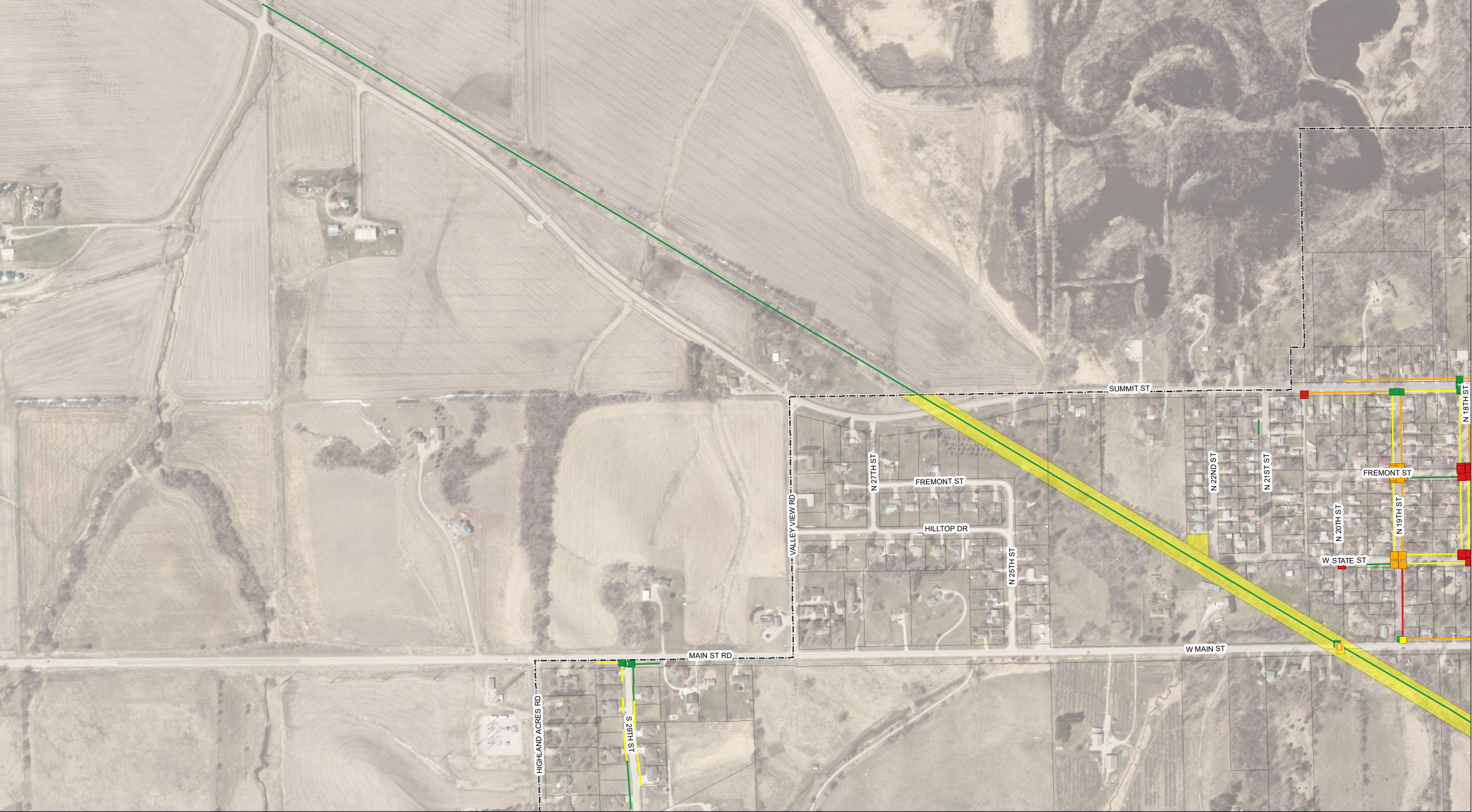


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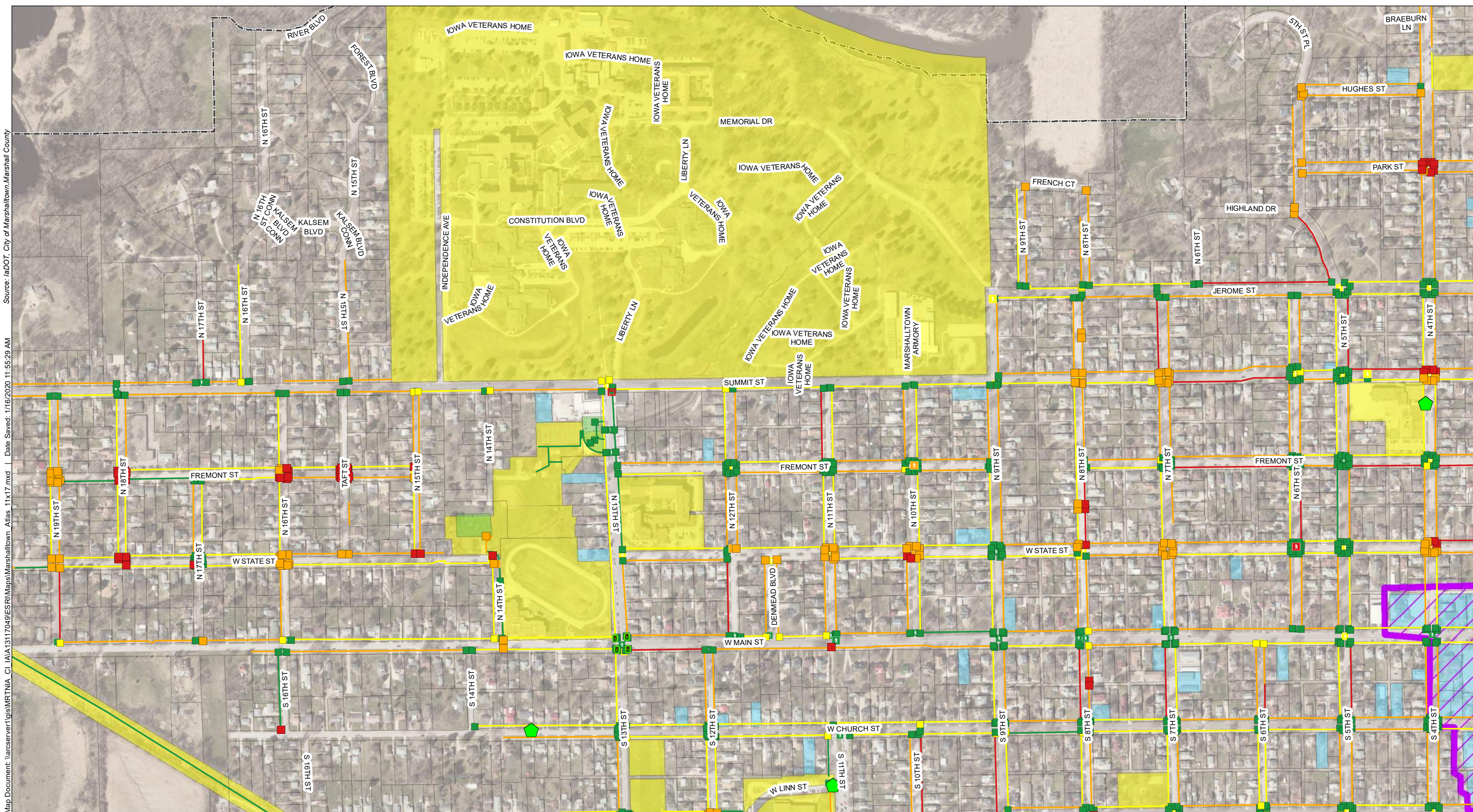
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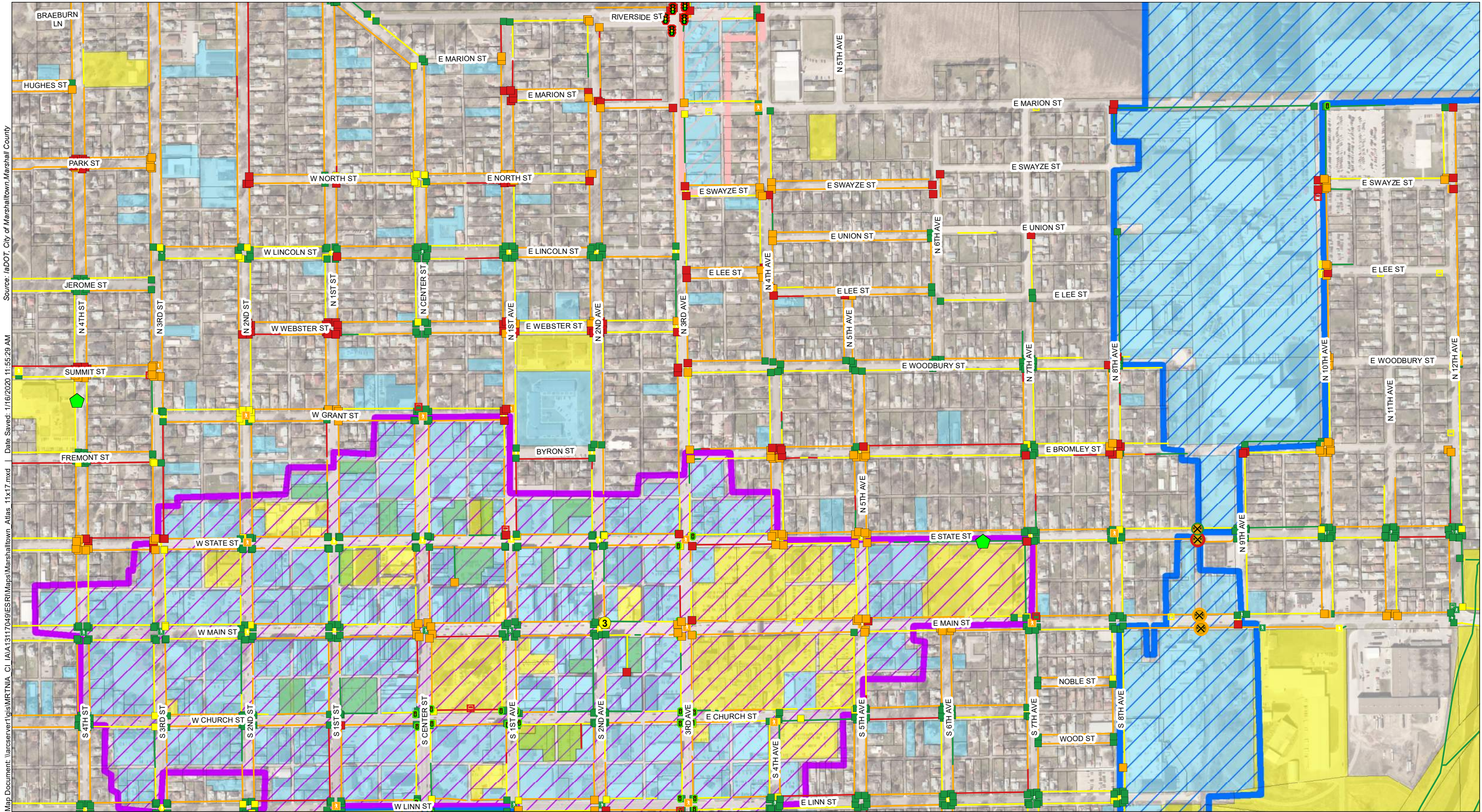


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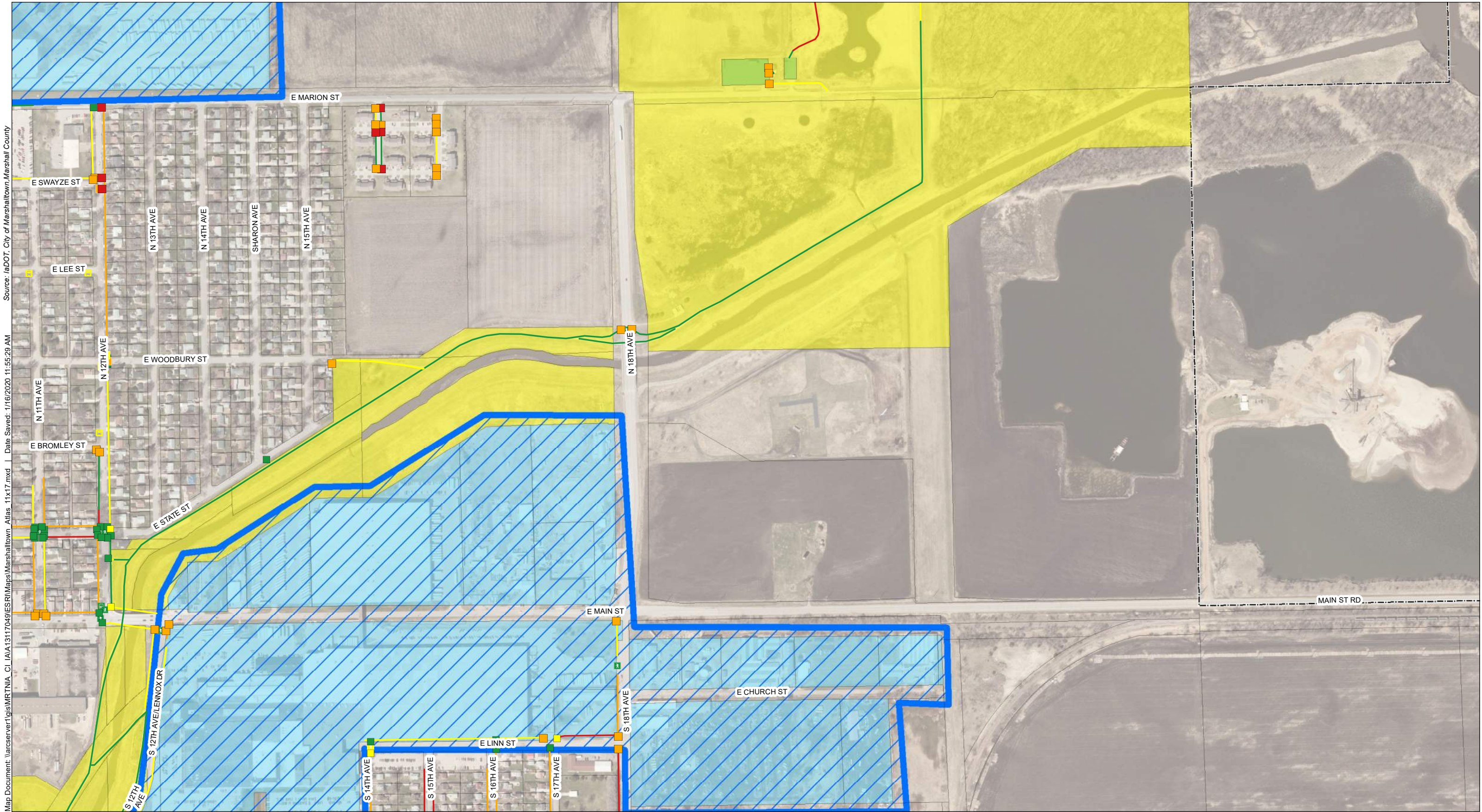


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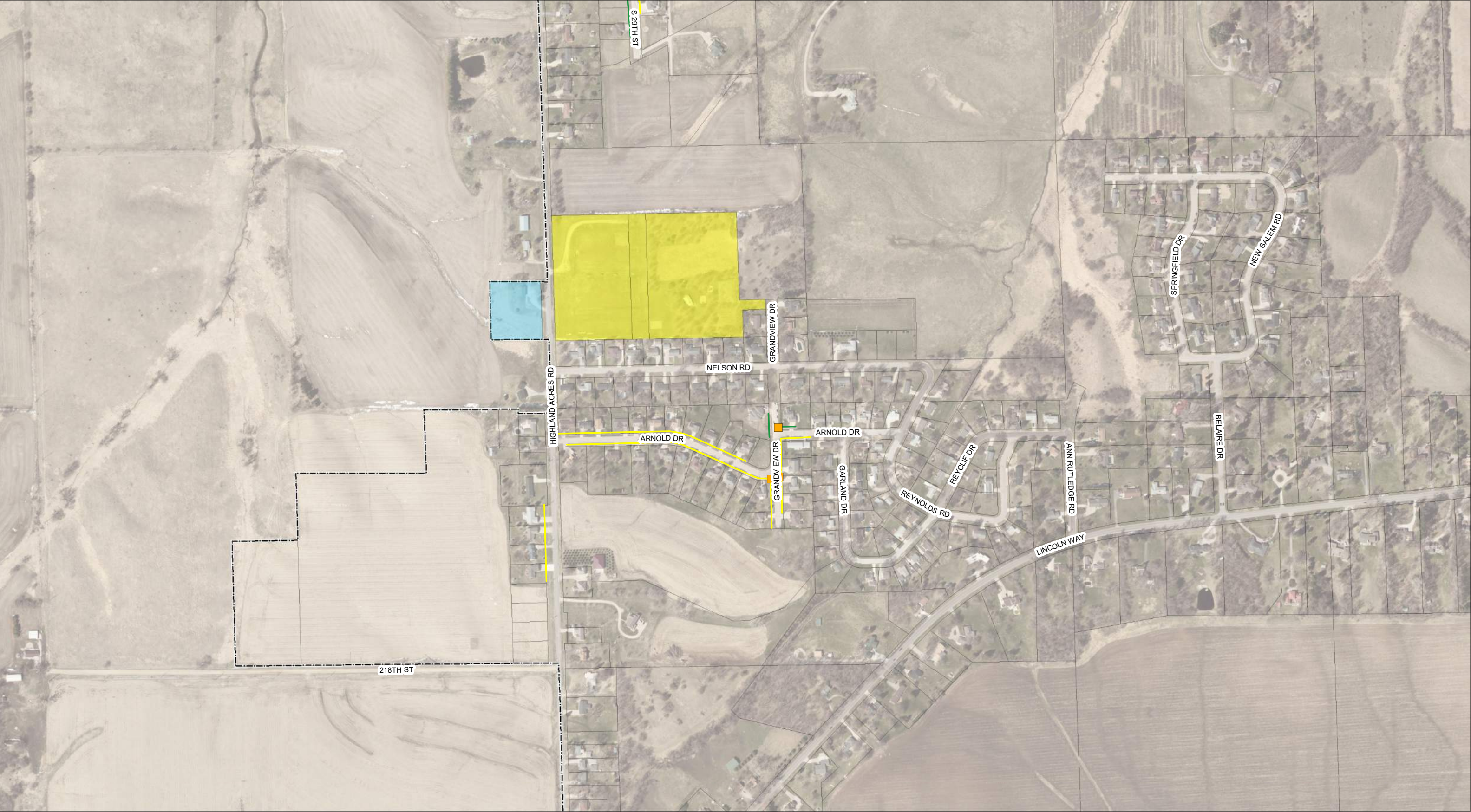
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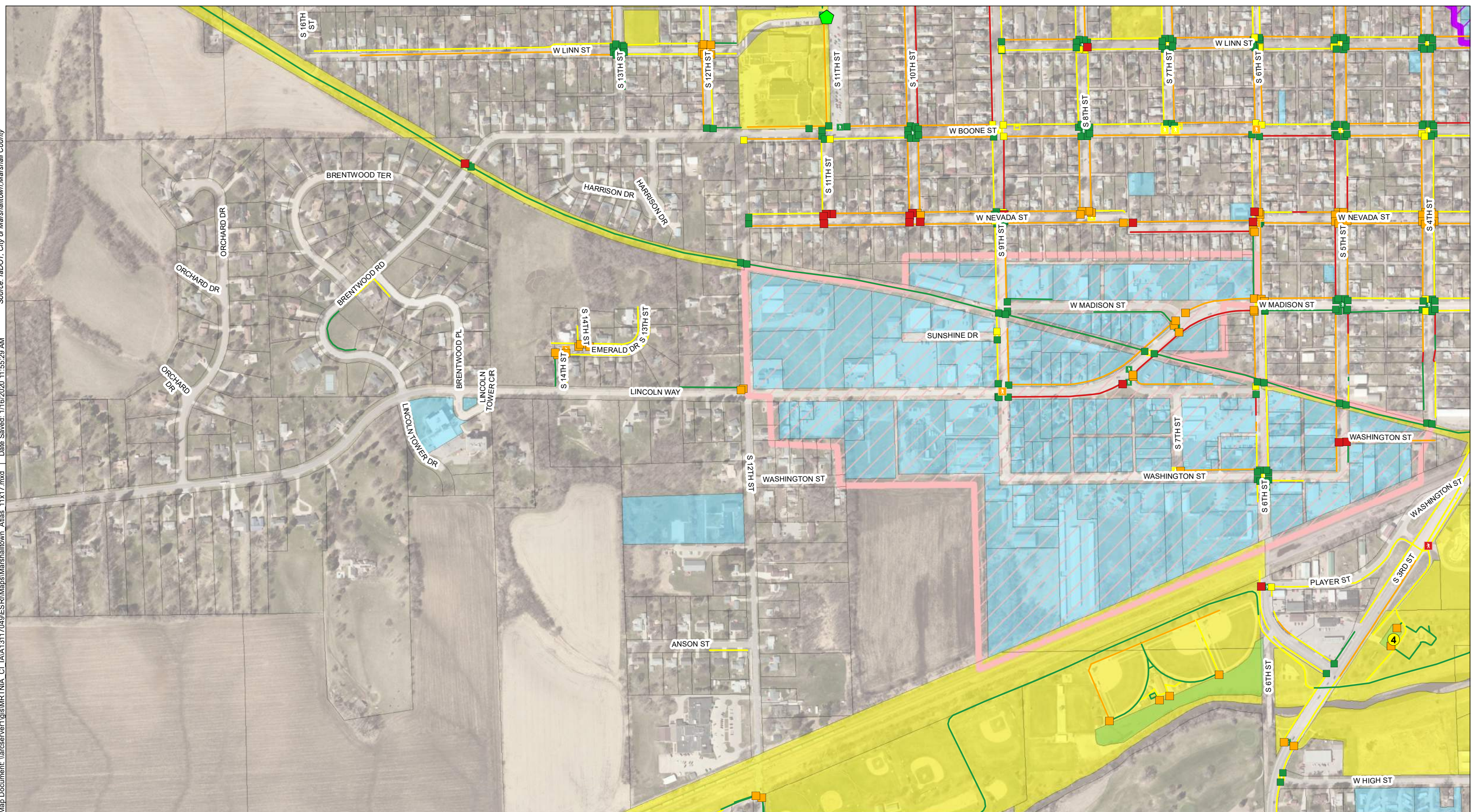
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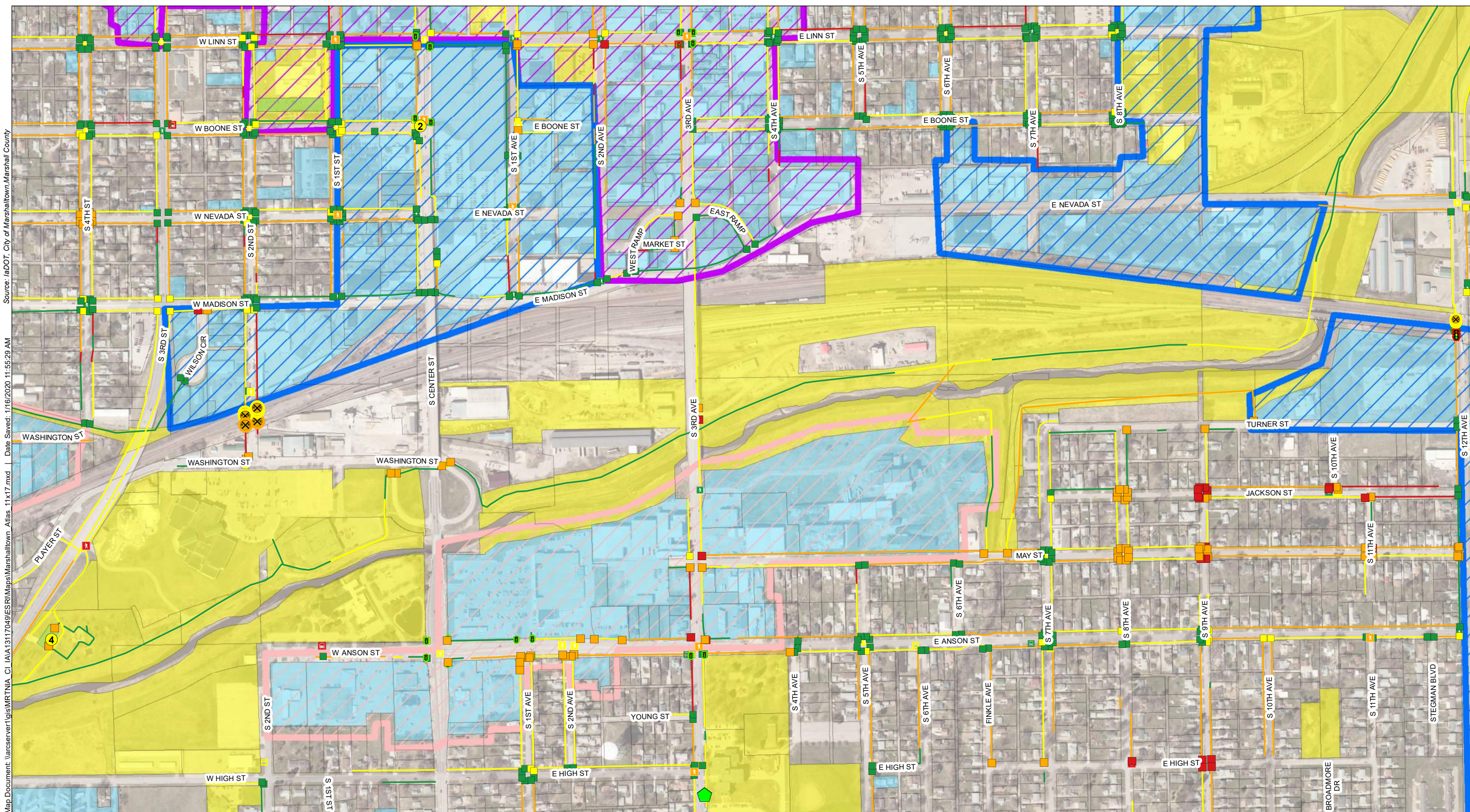
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- Figure B-8 -

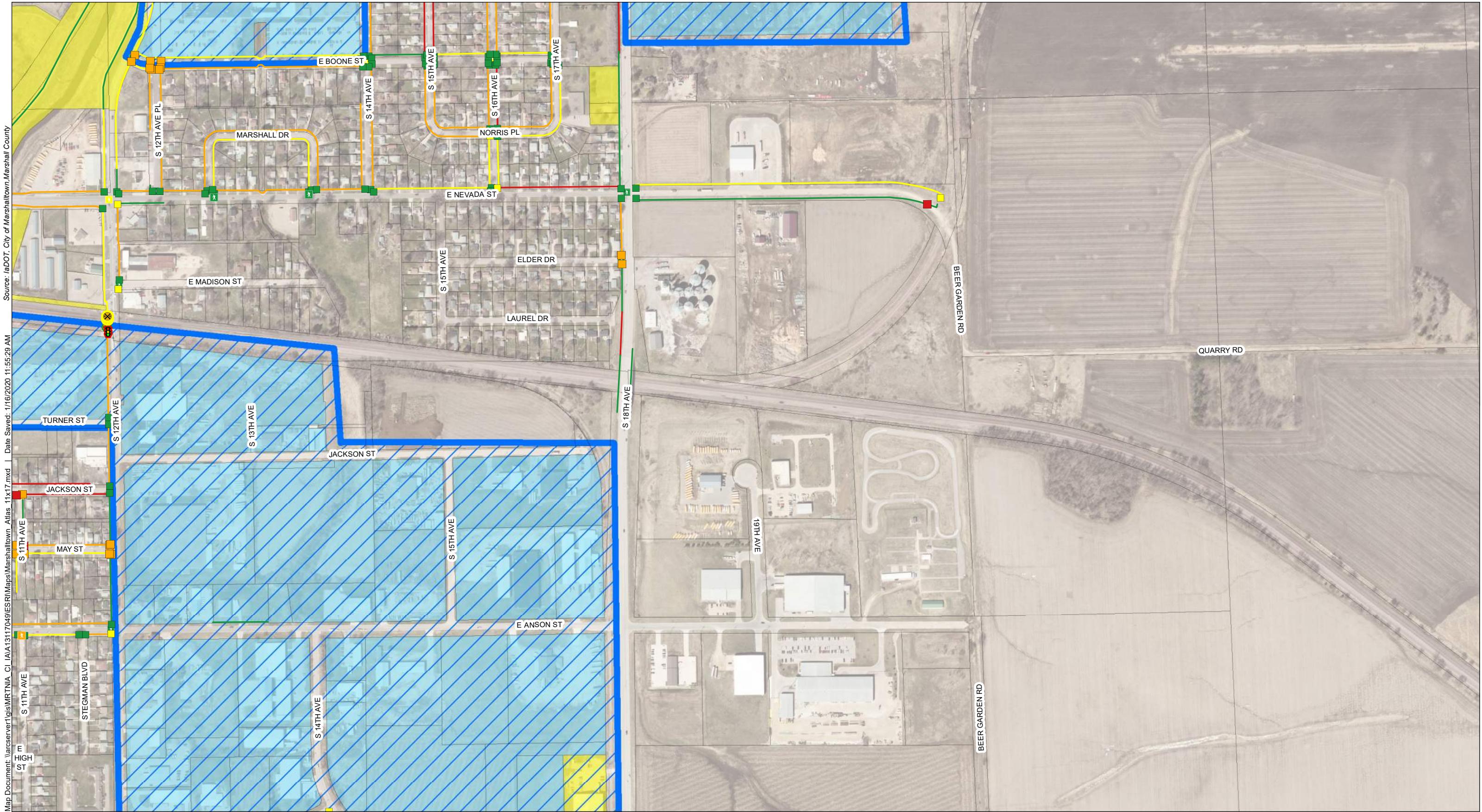


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- Figure B-10 -

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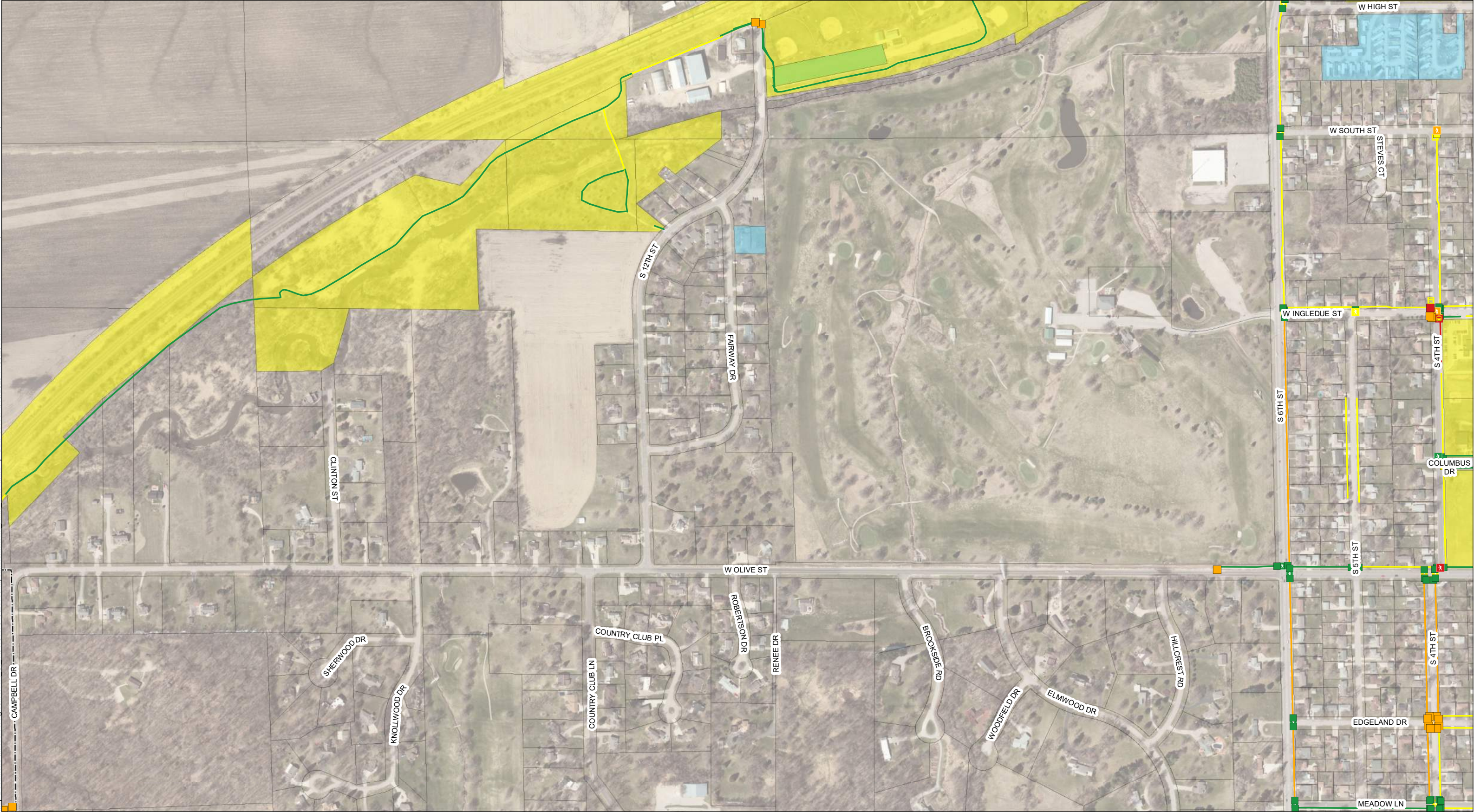


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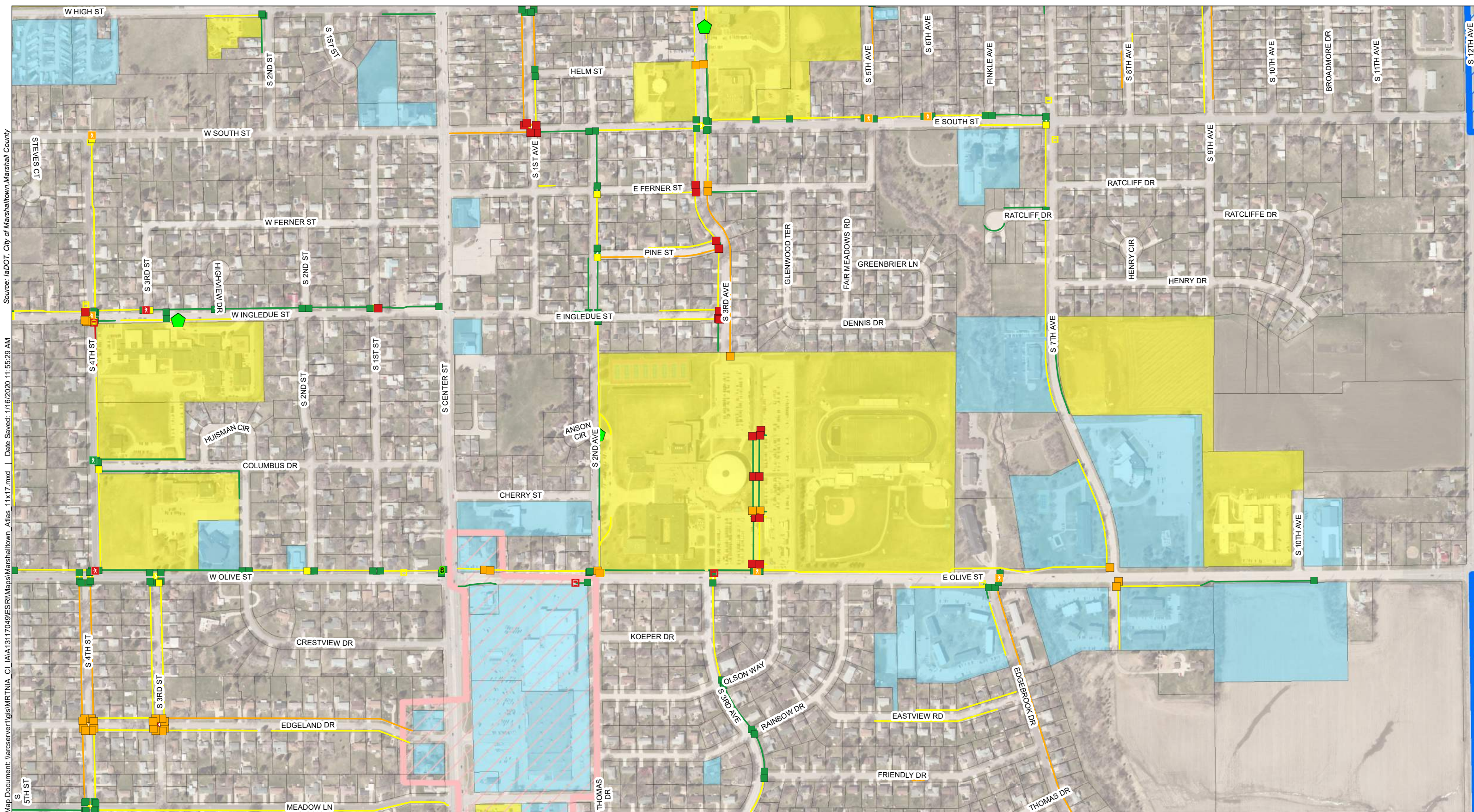


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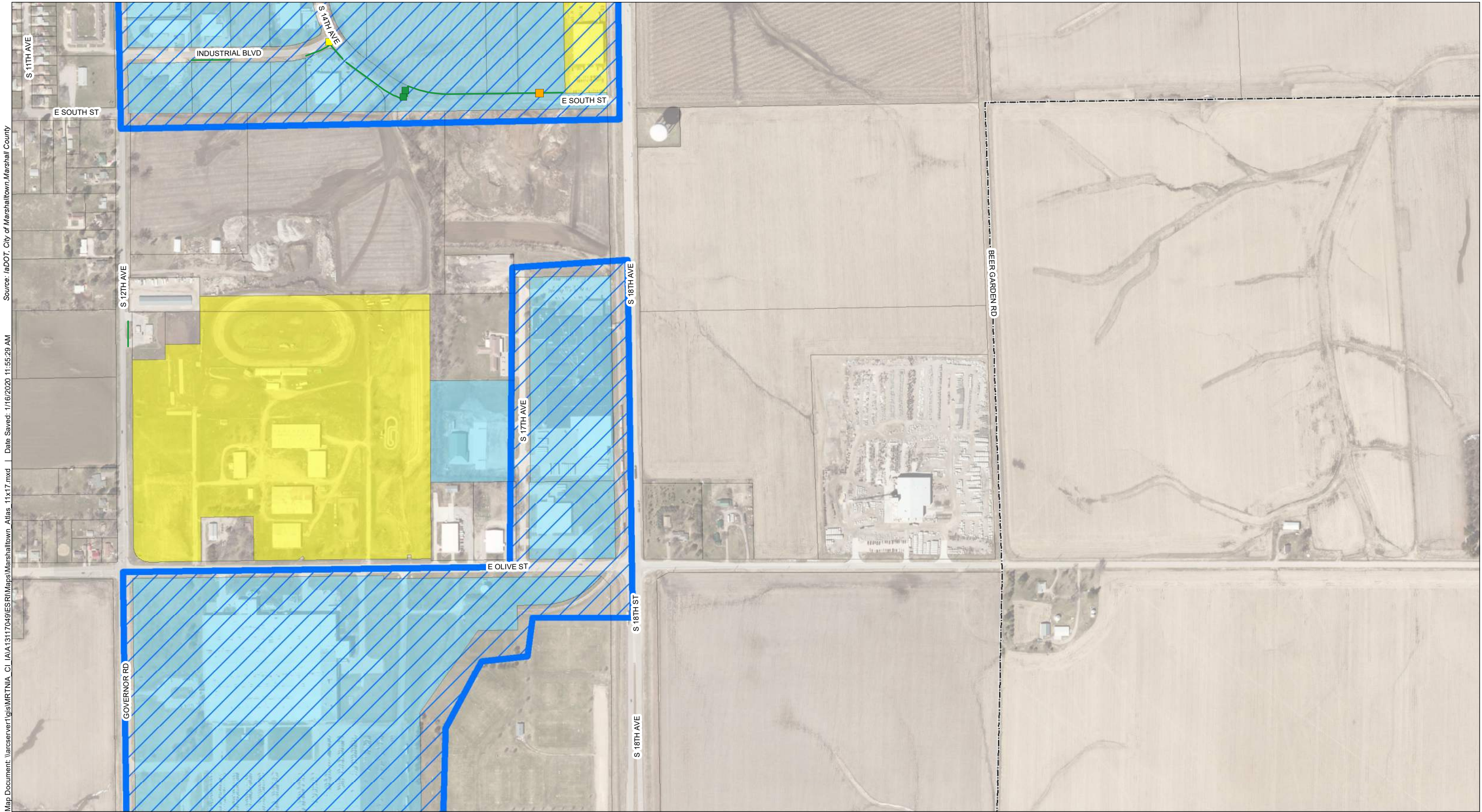


- Figure B-13 -

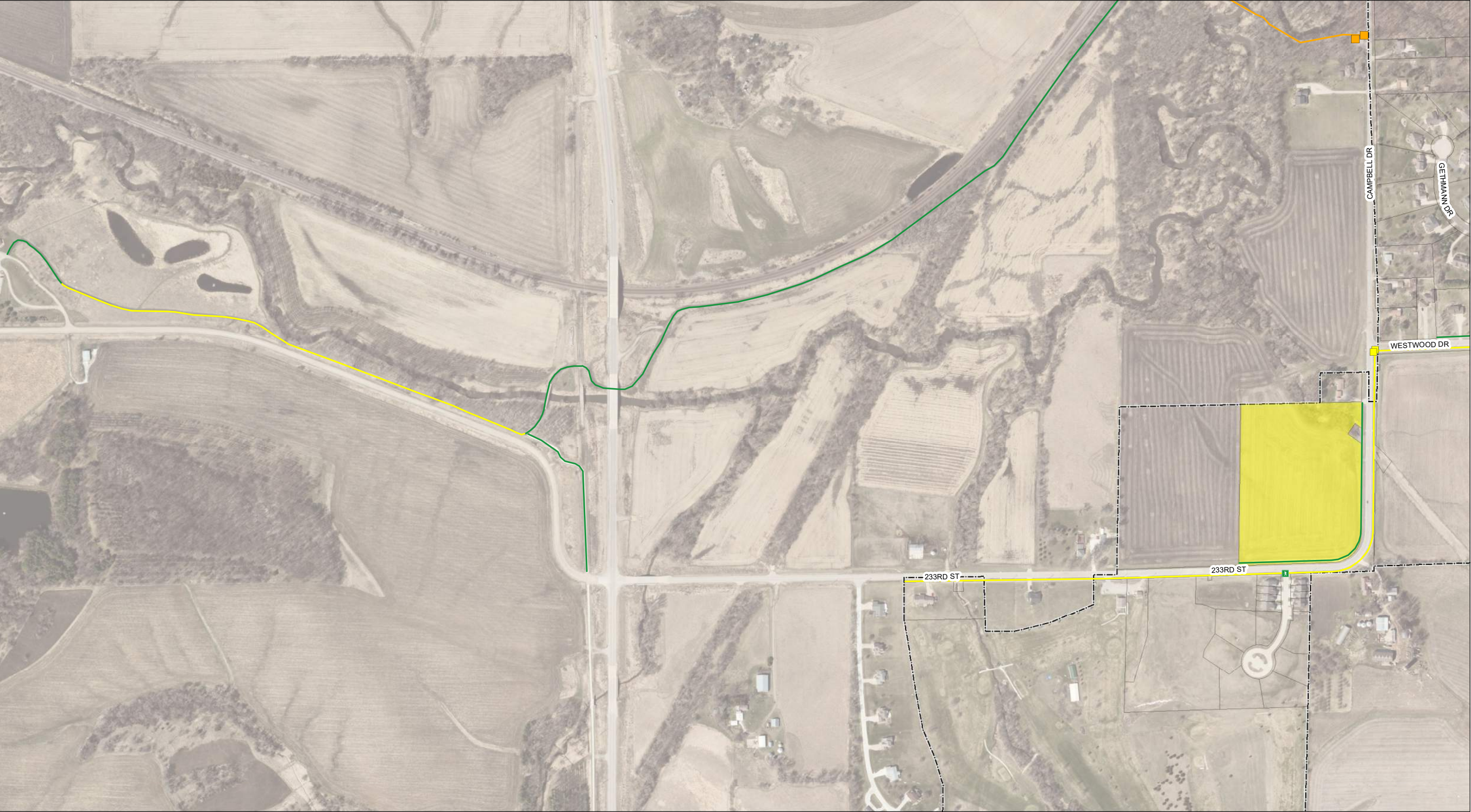


- Figure B-14 -

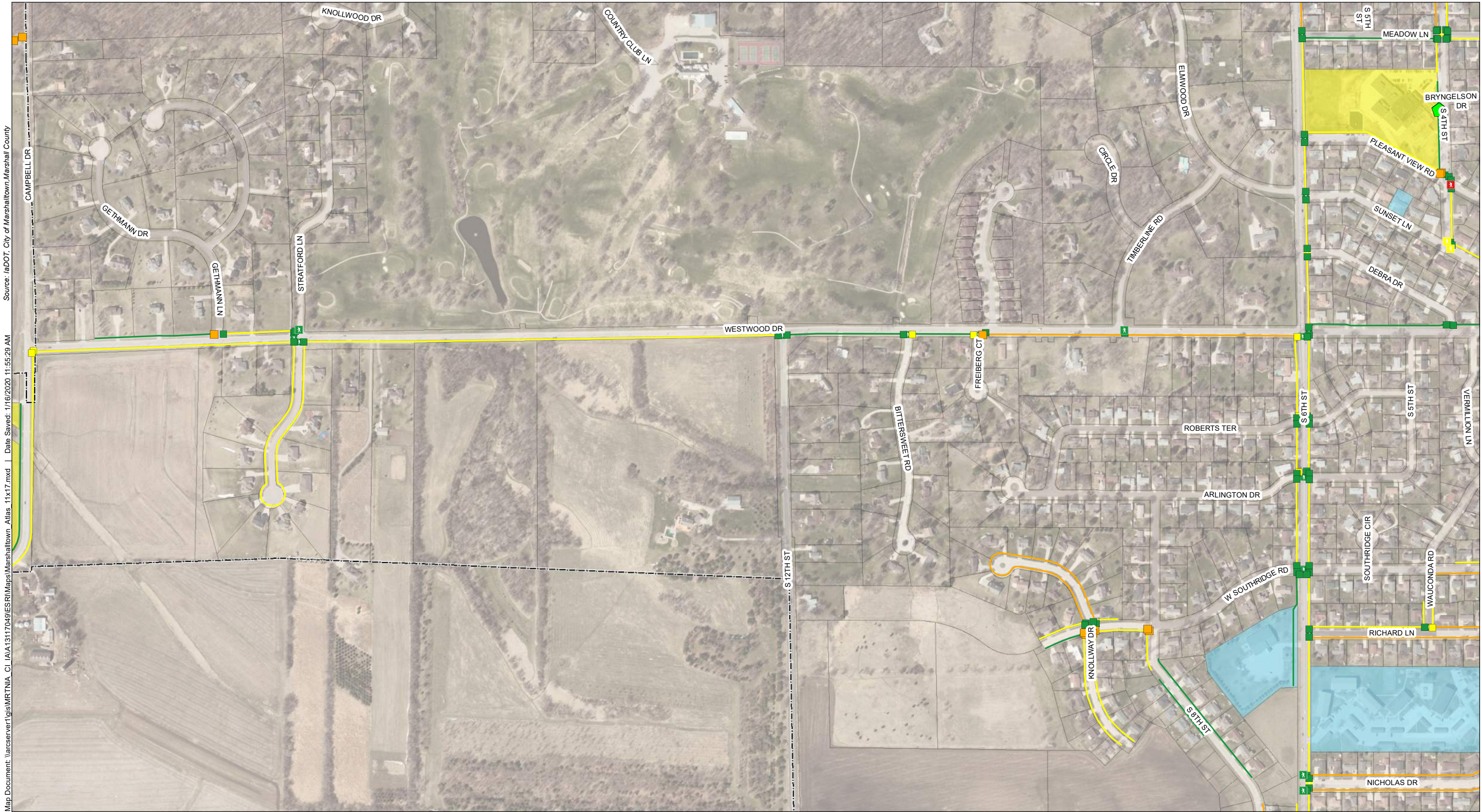
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Source: IADOT, City of Marshalltown, Marshall County



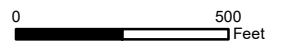
- Figure B-15 -

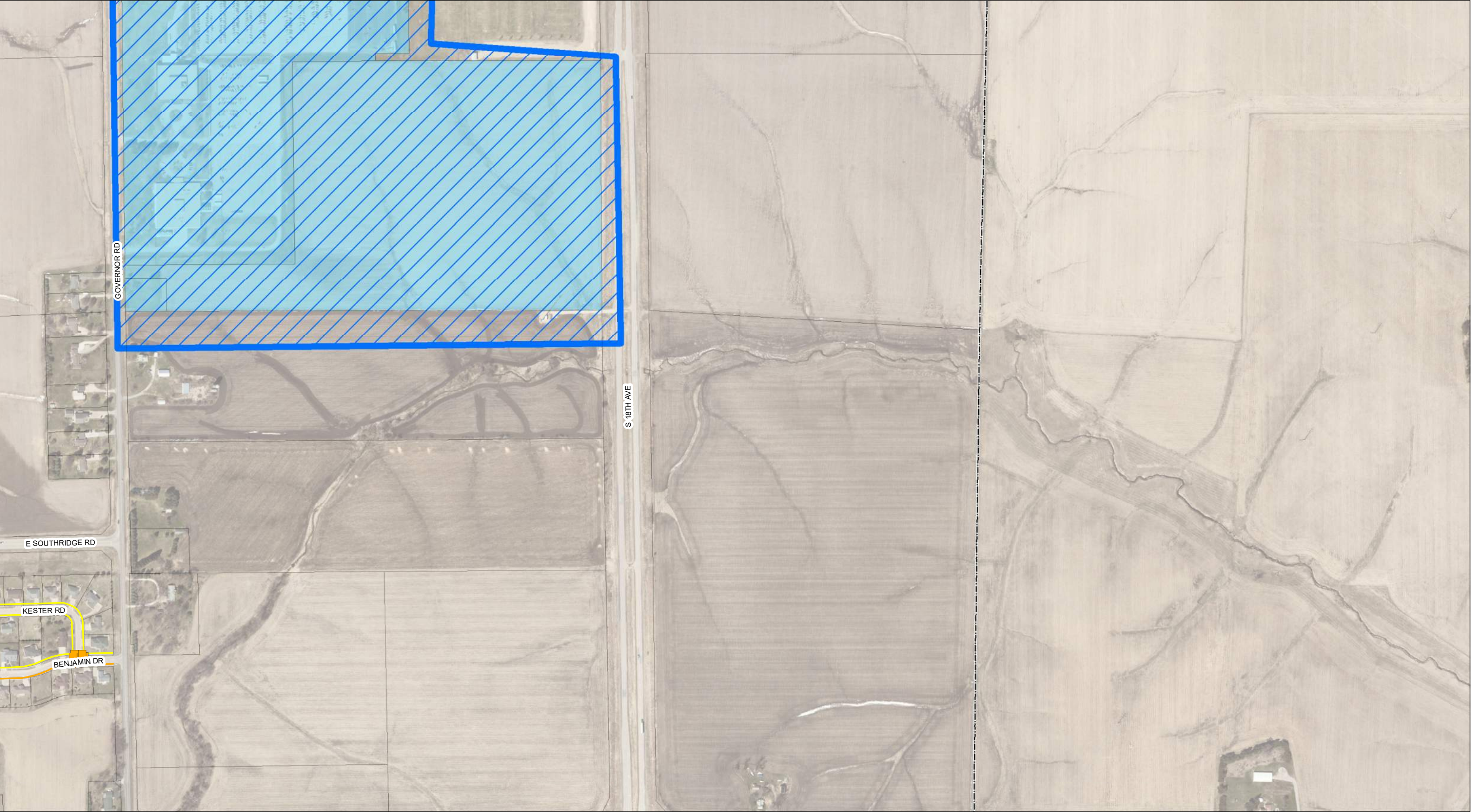


- Figure B-16 -

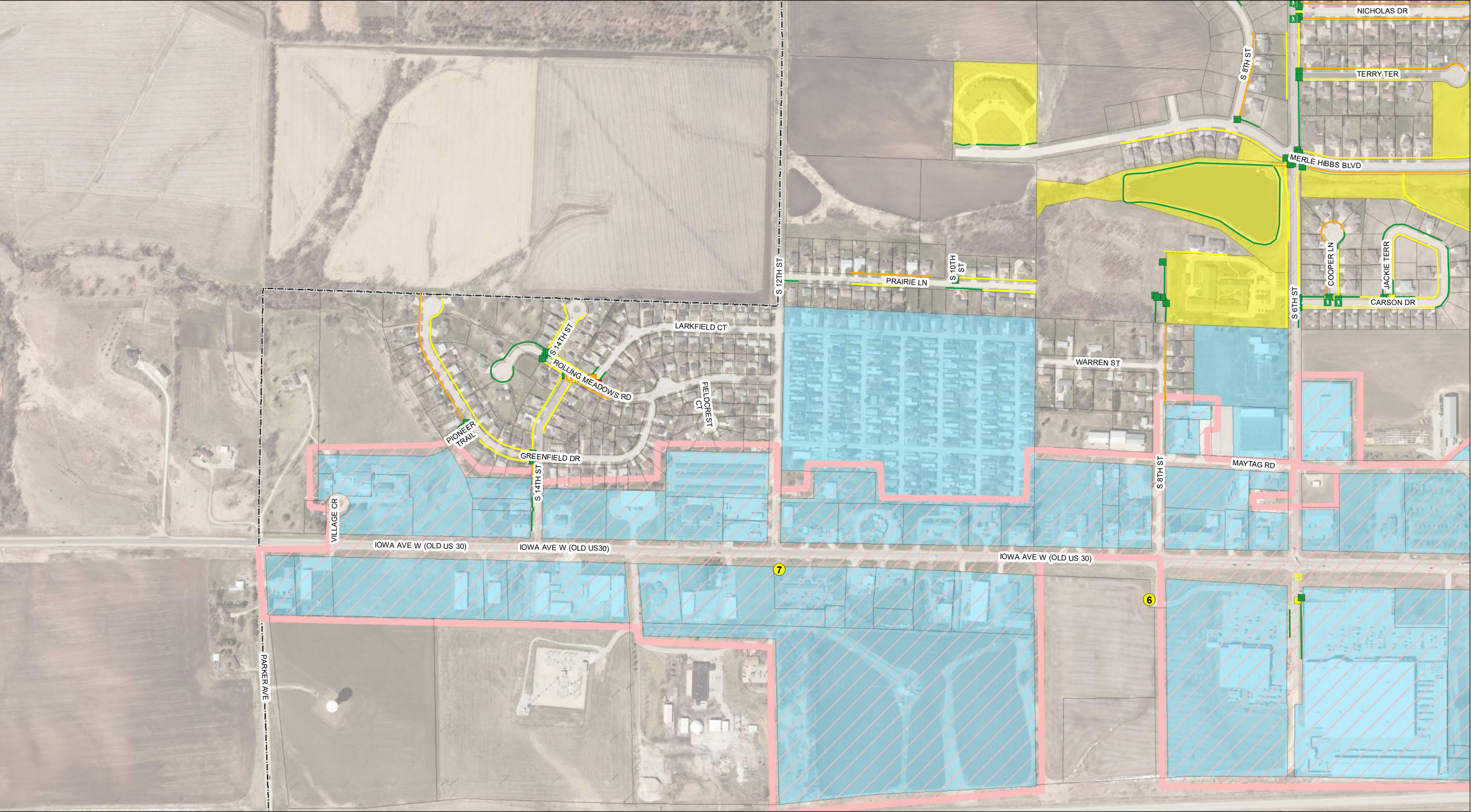


- Figure B-17 -



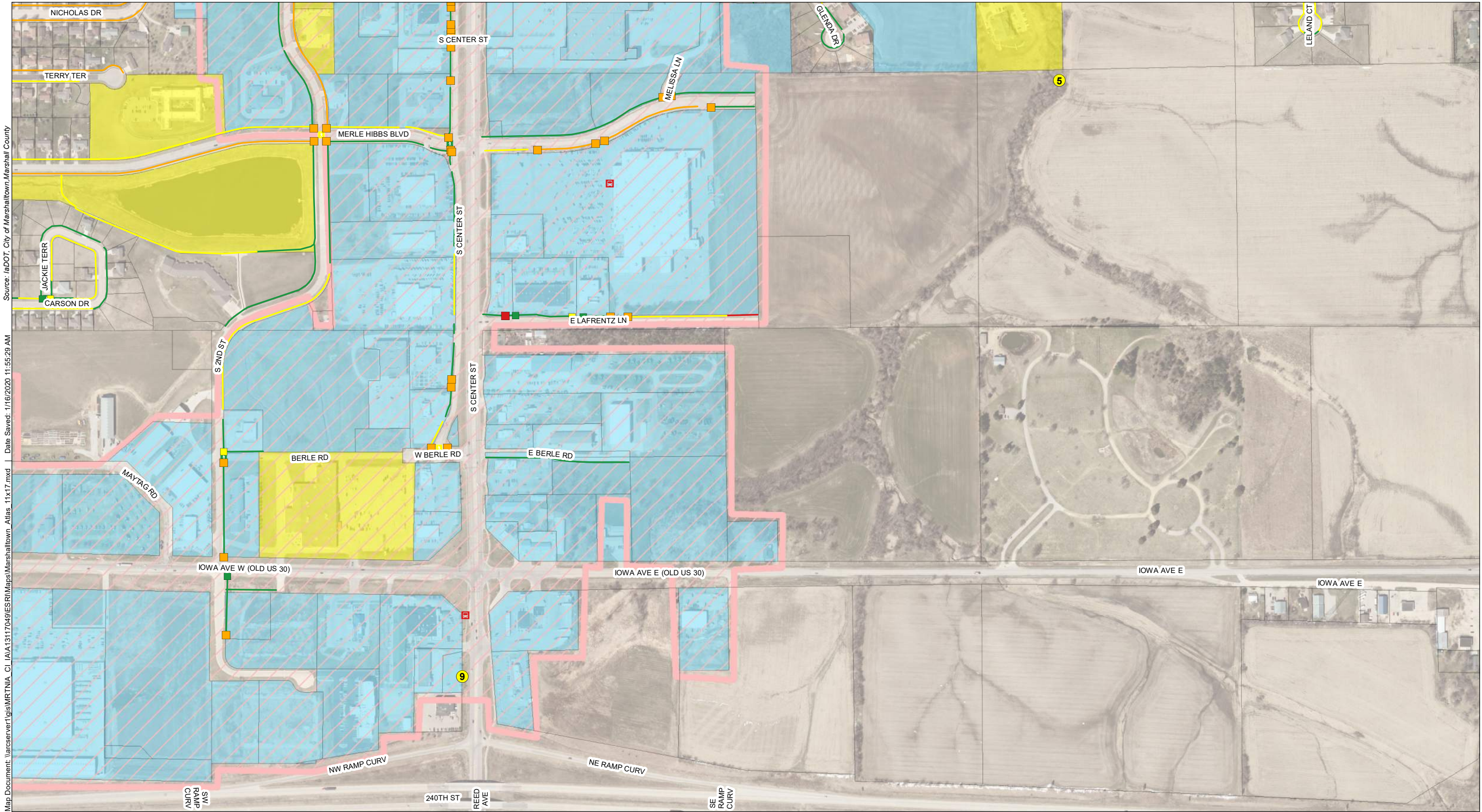


- Figure B-19 -



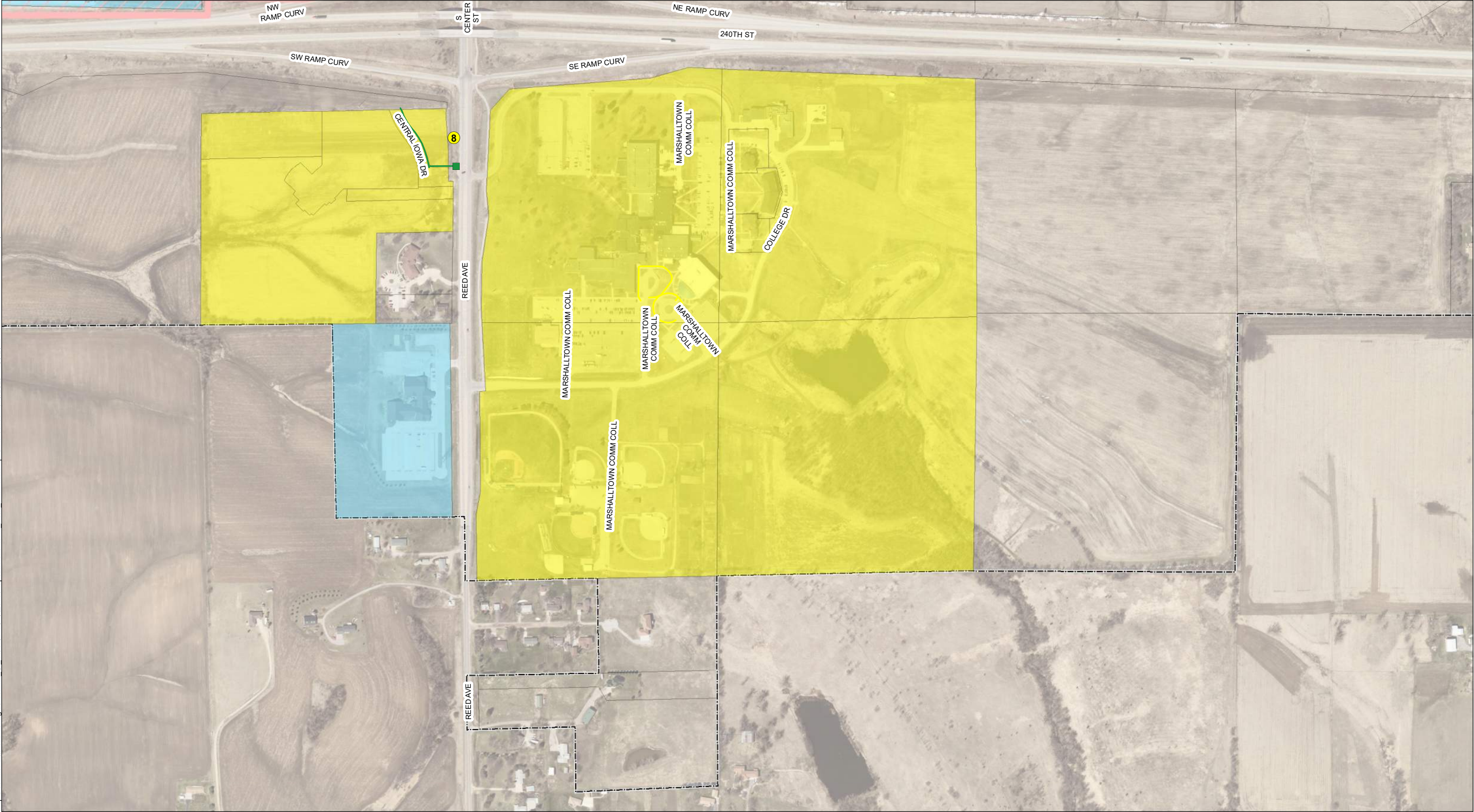
- Figure B-20 -

Source: laDOT, City of Marshalltown, Marshall County
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- Figure B-21 -

Map Document: \\arcserver1\gis\mrt\NIA_CI\IAA13117049\ESRI\Mapa\Marshalltown_Atlas_11x17.mxd | Date Saved: 1/16/2020 11:55:29 AM
Source: IADOT, City of Marshalltown, Marshall County



- Figure B-22 -

Table 3-A.2. City of Marshalltown Program Review - Implementation Plan											
Location	Land Use	Sidewalk Remediation Cost	Pedestrian Ramp Remediation Cost	Bus Stops Remediation Cost	Traffic Signal Remediation Cost*	Crosswalk Remediation Cost	Railroad Crossing Remediation Cost	Sidewalk/Trail Barriers Remediation Cost	Total Cost	Priority Level	Term
S 8TH AVE	Schools; Government Facilities; Housing complexes, including apartments	18338	36000	0	0	2000	0	3625	59963	1	Short-Term
W STATE ST	Schools; Government Facilities; Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments; Public Housing and Homeless Shelters	138747	156000	0	0	2500	0	14750	311997	1	Short-Term
E LINN ST	Schools; Churches and Places of Worship; Government Facilities; Housing complexes, including apartments	80737	20000	4000	0	2000	0	5250	111987	1	Short-Term
SUMMIT ST	Schools; Churches and Places of Worship; Government Facilities; Housing complexes, including apartments	177745	92000	0	0	500	0	6500	276745	1	Short-Term
E CHURCH ST	Schools; Churches and Places of Worship; Government Facilities; Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments	71506	0	4000	0	0	0	2500	78006	1	Short-Term
E MAIN ST	Schools; Bus Stop; Government Facilities; Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments; Parks	171891	36000	0	0	500	1000	3875	213266	1	Short-Term
S 5TH AVE	Schools; Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments; Public Housing and Homeless Shelters	75519	0	0	0	0	0	4125	79644	1	Short-Term
N 3RD AVE	Schools; Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments; Parks	77971	120000	0	37500	2500	0	2625	240596	1	Short-Term
TIMBER CREEK RECREATION AREA	Schools; Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments; Parks	0	0	0	0	0	0	250	250	1	Short-Term
E SOUTH ST	Schools; Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments	10229	4000	0	0	1000	0	1500	16729	1	Short-Term
PLAZA HTS RD	Schools; Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments	0	4000	0	0	0	0	1750	5750	1	Short-Term
3RD AVE	Schools; Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments	46742	8000	0	0	0	0	1625	56367	1	Short-Term
3RD AVE & E CHURCH ST	Schools; Hospitals, Health Clinics, and Medical Facilities (Public and Private)	0	0	0	0	0	0	125	125	1	Short-Term
S 4TH AVE	Schools; Churches and Places of Worship; Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments	8892	4000	0	0	500	0	2000	15392	1	Short-Term
N 5TH AVE	Schools; Churches and Places of Worship; Hospitals, Health Clinics, and Medical Facilities (Public and Private)	53446	56000	0	0	2500	0	1500	113446	1	Short-Term
THUNDERBIRD DR	Schools; Public Housing and Homeless Shelters	0	20000	0	0	0	0	750	20750	1	Short-Term
FREMONT ST	Schools; Housing complexes, including apartments; Public Housing and Homeless Shelters	153093	56000	0	0	1500	0	4500	215093	1	Short-Term
W CHURCH ST	Schools; Churches and Places of Worship; Housing complexes, including apartments; Parks; Public Housing and Homeless Shelters	119234	8000	0	0	0	0	10875	138109	1	Short-Term
S 6TH ST	Schools; Churches and Places of Worship; Housing complexes, including apartments; Parks; Public Housing and Homeless Shelters	133313	56000	0	0	4500	0	12500	206313	1	Short-Term
S 3RD ST	Schools; Churches and Places of Worship; Housing complexes, including apartments; Parks; Public Housing and Homeless Shelters	77609	32000	4000	0	2500	0	5625	121734	1	Short-Term
S 1ST AVE	Schools; Churches and Places of Worship; Government Facilities; Public Housing and Homeless Shelters	59983	56000	0	0	3000	0	6000	124983	1	Short-Term
S CENTER ST	Schools; Churches and Places of Worship; Government Facilities; Parks; Public Housing and Homeless Shelters	48675	96000	12000	0	1000	0	2750	160425	1	Short-Term
N 2ND ST	Schools; Churches and Places of Worship; Government Facilities; Housing complexes, including apartments; Public Housing and Homeless Shelters	140652	32000	0	0	4000	0	1375	178027	1	Short-Term
Short-Term Projects (2020-2029) Totals		\$1,664,322	\$892,000	\$24,000	\$37,500	\$30,500	\$1,000	\$96,375	\$2,745,697	-	Short-Term
S 8TH ST	Schools; Housing complexes, including apartments; Public Housing and Homeless Shelters	70220	32000	0	0	1000	0	1375	104595	1	Mid-Term
N 13TH ST	Schools; Housing complexes, including apartments; Parks; Public Housing and Homeless Shelters	9595	0	4000	0	0	0	1750	15345	1	Mid-Term
N 1ST ST	Schools; Churches and Places of Worship; Government Facilities; Housing complexes, including apartments; Public Housing and Homeless Shelters	114695	36000	0	0	0	0	1375	152070	1	Mid-Term
S 2ND ST	Schools; Churches and Places of Worship; Government Facilities; Housing complexes, including apartments; Libraries; Parks; Public Housing and Homeless Shelters	111217	40000	0	0	2500	3000	3250	159967	1	Mid-Term
E STATE ST	Schools; Churches and Places of Worship; Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments; Public Housing and Homeless Shelters	144062	64000	4000	0	4000	1000	3750	220812	1	Mid-Term
W BOONE ST	Schools; Housing complexes, including apartments; Libraries	148313	0	0	0	1000	0	6000	155313	1	Mid-Term
W LINN ST	Schools; Churches and Places of Worship; Housing complexes, including apartments; Libraries; Parks	150277	4000	0	0	2000	0	10000	166277	1	Mid-Term
N 14TH ST	Schools; Parks	0	16000	0	0	1000	0	500	17500	1	Mid-Term
MAY ST	Schools; Parks	125123	116000	0	0	4500	0	4375	249998	1	Mid-Term
LINN CREEK GREENBELT PARKWAY	Schools; Parks	124044	0	0	0	0	0	6375	130419	1	Mid-Term
N 4TH ST	Schools; Parks	133504	40000	0	0	2000	0	1875	177379	1	Mid-Term
S 12TH ST	Schools; Housing complexes, including apartments; Parks	29003	40000	0	0	2000	0	2125	73128	1	Mid-Term
S 3RD AVE	Schools; Housing complexes, including apartments; Parks	70342	132000	8000	0	3000	0	6875	220217	1	Mid-Term
N 3RD ST	Schools; Housing complexes, including apartments; Parks	164561	68000	4000	0	1500	0	375	238436	1	Mid-Term
MELODY LN	Schools; Housing complexes, including apartments; Parks	0	0	0	0	0	0	1250	1250	1	Mid-Term
S 5TH ST	Schools; Housing complexes, including apartments; Parks	113957	8000	0	0	0	0	3375	125332	1	Mid-Term
N 9TH AVE	Schools; Housing complexes, including apartments	0	12000	0	0	500	0	1625	14125	1	Mid-Term

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W LINCOLN ST	Schools; Housing complexes, including apartments	31021	4000	0	0	0	0	1875	36896	1	Mid-Term
LINCOLN WAY	Schools; Housing complexes, including apartments	.	12000	0	0	2000	0	0	63571	1	Mid-Term
W GRANT ST	Schools; Housing complexes, including apartments	49088	28000	4000	0	2500	0	2250	85838	1	Mid-Term
N 9TH ST	Schools; Housing complexes, including apartments	62825	4000	0	0	500	0	2625	69950	1	Mid-Term
E SOUTHRIDGE RD	Schools; Housing complexes, including apartments	16655	36000	0	0	500	0	1375	54530	1	Mid-Term
N 7TH AVE	Schools; Housing complexes, including apartments	44997	8000	4000	0	2000	0	1000	59997	1	Mid-Term
S 6TH AVE	Schools; Housing complexes, including apartments	57494	16000	0	0	1000	0	875	75369	1	Mid-Term
EDGEBROOK DR	Schools; Housing complexes, including apartments	69417	28000	0	0	1500	0	1875	100792	1	Mid-Term
W MADISON ST	Schools; Housing complexes, including apartments	37208	24000	0	0	500	0	2250	63958	1	Mid-Term
Mid-Term Projects (2030-2039) Totals		\$1,877,618	\$768,000	\$28,000	\$0	\$35,500	\$4,000	\$70,375	\$2,833,064	-	Mid-Term
E LEE ST	Schools; Housing complexes, including apartments	54630	40000	0	0	2500	0	1625	98755	1	Long-Term
W WEBSTER ST	Schools; Housing complexes, including apartments	36133	0	0	0	0	0	375	36508	1	Long-Term
N 4TH AVE	Schools; Housing complexes, including apartments	62539	44000	0	0	1500	0	4500	112539	1	Long-Term
N 7TH ST	Schools; Housing complexes, including apartments	56663	0	0	0	0	0	1375	58038	1	Long-Term
N 8TH AVE	Schools; Housing complexes, including apartments	33136	8000	0	0	0	0	1375	42511	1	Long-Term
S 4TH ST	Schools; Churches and Places of Worship; Housing complexes, including apartments	142953	84000	4000	0	9000	0	3750	243703	1	Long-Term
S 2ND AVE	Schools; Churches and Places of Worship; Housing complexes, including apartments	76594	48000	0	0	3000	0	2875	130469	1	Long-Term
W MAIN ST	Schools; Churches and Places of Worship; Housing complexes, including apartments	203219	40000	0	0	1000	0	8375	252594	1	Long-Term
E OLIVE ST	Schools; Churches and Places of Worship; Housing complexes, including apartments	0	20000	4000	0	0	0	2250	26250	1	Long-Term
E BROMLEY ST	Schools; Churches and Places of Worship; Housing complexes, including apartments	52597	92000	0	0	6000	0	3125	153722	1	Long-Term
S 7TH AVE	Schools; Churches and Places of Worship; Housing complexes, including apartments	50088	12000	0	0	1000	0	5875	68963	1	Long-Term
W NEVADA ST	Schools; Churches and Places of Worship; Housing complexes, including apartments	182793	84000	0	0	5000	0	3500	275293	1	Long-Term
W OLIVE ST	Schools; Churches and Places of Worship	0	4000	0	0	0	0	1500	5500	1	Long-Term
E ANSON ST	Schools; Churches and Places of Worship	124693	16000	0	0	500	0	6375	147568	1	Long-Term
N 5TH ST	Schools; Churches and Places of Worship	71638	28000	0	0	0	0	2500	102138	1	Long-Term
WESTWOOD DR	Schools	75411	16000	0	0	1000	0	875	93286	1	Long-Term
S 13TH ST	Schools	10289	0	0	0	0	0	1000	11289	1	Long-Term
WOOD ST	Schools	19328	0	0	0	0	0	0	19328	1	Long-Term
PINE ST	Schools	15460	8000	0	0	0	0	250	23710	1	Long-Term
YOUNG ST	Schools	0	0	0	0	0	0	250	250	1	Long-Term
W INGLEDUE ST	Schools	0	4000	0	0	1000	0	1250	6250	1	Long-Term
S 9TH AVE	Schools	37289	52000	0	0	2000	0	1875	93164	1	Long-Term
FRIENDLY DR	Schools	1470	0	0	0	0	0	0	1470	1	Long-Term
PLEASANT VIEW RD	Schools	0	0	0	0	0	0	375	375	1	Long-Term
N 10TH ST	Schools	19145	32000	0	0	4000	0	3750	58895	1	Long-Term
E WOODBURY ST & N 8TH AVE	Schools	0	0	0	0	0	0	250	250	1	Long-Term
N 6TH AVE	Schools	17714	12000	0	0	0	0	0	29714	1	Long-Term
S 3RD AVE PL	Schools	0	8000	0	0	0	0	875	8875	1	Long-Term
CRAIG CIR	Schools	0	0	0	0	0	0	125	125	1	Long-Term
NOBLE ST	Schools	19213	0	0	0	0	0	0	19213	1	Long-Term
TRAVIS DR	Schools	0	0	0	0	500	0	125	625	1	Long-Term
EASTVIEW RD	Schools	0	0	0	0	0	0	1000	1000	1	Long-Term
W HIGH ST	Schools	0	0	0	0	0	0	375	375	1	Long-Term
MEADOW LN	Schools	0	0	0	0	0	0	375	375	1	Long-Term
COLUMBUS DR	Schools	0	0	0	0	0	0	125	125	1	Long-Term
E FERNER ST	Schools	0	0	0	0	0	0	250	250	1	Long-Term
SUNSET LN	Schools	0	0	0	0	0	0	375	375	1	Long-Term
N 12TH ST	Schools	38571	4000	0	0	500	0	1125	44196	1	Long-Term
FINKLE AVE	Schools	14488	4000	0	0	0	0	0	18488	1	Long-Term
E HIGH ST	Schools	0	8000	0	0	500	0	1125	9625	1	Long-Term
N 16TH ST	Schools	9702	60000	0	0	2500	0	1875	74077	1	Long-Term
E WOODBURY ST	Schools	40422	28000	0	0	4500	0	4125	77047	1	Long-Term
TAFT ST	Schools	2780	40000	0	0	2000	0	0	44780	1	Long-Term
N 15TH ST	Schools	45221	16000	0	0	500	0	875	62596	1	Long-Term
S 14TH ST	Schools	1954	32000	0	0	1500	0	1875	37329	1	Long-Term
EMERALD DR	Schools	0	0	0	0	0	0	1625	1625	1	Long-Term
GLENDA DR	Schools	4415	0	0	0	0	0	125	4540	1	Long-Term
N 6TH ST	Schools	59332	0	0	0	2000	0	1500	62832	1	Long-Term
JEROME ST	Schools	56249	0	0	0	0	0	3125	59374	1	Long-Term
DENMEAD BLVD	Schools	19429	0	0	0	0	0	0	19429	1	Long-Term
BAILEY DR	Schools	0	0	0	0	0	0	750	750	1	Long-Term
N 11TH ST	Schools	29326	0	0	0	0	0	2625	31951	1	Long-Term
S 11TH ST	Schools	13091	0	0	0	0	0	375	13466	1	Long-Term
S 10TH ST	Schools	61868	0	0	0	0	0	0	61868	1	Long-Term

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Location	Land Use	Sidewalk Remediation Cost	Pedestrian Ramp Remediation Cost	Bus Stops Remediation Cost	Traffic Signal Remediation Cost*	Crosswalk Remediation Cost	Railroad Crossing Remediation Cost	Sidewalk/Trail Barriers Remediation Cost	Total Cost	Priority Level	Term
PARK ST	Schools	47482	0	0	0	0	0	0	47482	1	Long-Term
JOAN TERR	Schools	0	0	0	0	1500	0	1250	2750	1	Long-Term
EDGELAND DR	Schools	32146	0	0	0	0	0	1750	33896	1	Long-Term
DUBOIS CIR	Schools	0	0	0	0	0	0	750	750	1	Long-Term
E INGLEDUE ST	Schools	0	0	0	0	0	0	750	750	1	Long-Term
S 9TH ST	Schools	57427	0	0	0	0	0	4375	61802	1	Long-Term
NICHOLAS DR	Government Facilities; Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments	69193	0	0	0	0	0	1000	70193	1	Long-Term
E BOONE ST	Churches and Places of Worship; Government Facilities; Housing complexes, including apartments; Parks	73597	32000	0	0	0	0	5750	111347	1	Long-Term
N CENTER ST	Churches and Places of Worship; Government Facilities; Housing complexes, including apartments	154481	0	0	0	0	0	4125	158606	1	Long-Term
MERLE HIBBS BLVD	Hospitals, Health Clinics, and Medical Facilities (Public and Private); Parks; Public Housing and Homeless Shelters	67656	24000	0	0	0	0	2875	94531	1	Long-Term
N 1ST AVE	Hospitals, Health Clinics, and Medical Facilities (Public and Private); Housing complexes, including apartments; Public Housing and Homeless Shelters	97314	92000	0	0	2000	0	3000	194314	1	Long-Term
BERLE RD	Hospitals, Health Clinics, and Medical Facilities (Public and Private)	0	0	0	0	0	0	125	125	1	Long-Term
TERRY TER	Hospitals, Health Clinics, and Medical Facilities (Public and Private)	21713	0	0	0	0	0	1000	22713	1	Long-Term
W SOUTHRIDGE RD	Public Housing and Homeless Shelters	25631	44000	0	0	0	0	1875	71506	1	Long-Term
CAMPBELL DR / 233RD ST	Public Housing and Homeless Shelters	0	0	0	0	0	0	500	500	1	Long-Term
GLENWOOD PLACE	Public Housing and Homeless Shelters	0	0	0	0	0	0	2000	2000	1	Long-Term
N 2ND AVE	Housing complexes, including apartments; Public Housing and Homeless Shelters	87318	76000	0	0	2500	0	3250	169068	1	Long-Term
E WEBSTER ST	Housing complexes, including apartments; Public Housing and Homeless Shelters	20248	0	0	0	0	0	2250	22498	1	Long-Term
S 1ST ST	Churches and Places of Worship; Housing complexes, including apartments; Libraries	65580	0	0	0	0	0	625	66205	1	Long-Term
S 7TH ST	Housing complexes, including apartments; Parks	29923	0	0	0	0	0	3125	33048	1	Long-Term
E MARION ST	Housing complexes, including apartments; Parks	26826	8000	0	0	0	0	1125	35951	1	Long-Term
Mega-10 Park	Parks	0	0	0	0	0	0	3500	3500	1	Long-Term
GOLDFINCH RECREATION AREA	Parks	0	0	0	0	0	0	1375	1375	1	Long-Term
GOVERNOR RD	Parks	29090	0	0	7500	0	0	875	37465	1	Long-Term
WASHINGTON ST	Parks	21226	16000	0	0	500	0	375	38101	1	Long-Term
SOFTBALL COMPLEX	Parks	25563	0	0	0	0	0	875	26438	1	Long-Term
E NEVADA ST	Parks	78673	4000	0	0	1000	0	1125	84798	1	Long-Term
RIVERVIEW PARK	Parks	10337	8000	0	0	0	0	625	18962	1	Long-Term
S 12TH AVE	Parks	0	0	0	0	0	0	375	375	1	Long-Term
S 12TH AVE/LENNOX DR	Parks	1865	0	0	0	0	0	875	2740	1	Long-Term
N 12TH AVE	Parks	44099	20000	0	0	1000	0	1625	66724	1	Long-Term
S 18TH AVE	Parks	29957	8000	0	0	0	0	2875	40832	1	Long-Term
WOODLAND ST	Parks	0	0	0	0	0	0	125	125	1	Long-Term
W ANSON ST		8768	0	4000	0	0	0	125	12893	1	Long-Term
CAMPBELL DR		0	8000	0	0	0	0	0	8000	1	Long-Term
TURNER ST		0	4000	0	0	0	0	125	4125	1	Long-Term
BRENTWOOD RD		0	4000	0	0	0	0	500	4500	1	Long-Term
HUGHES ST	Housing complexes, including apartments	29916	0	0	0	0	0	0	29916	2	Long-Term
E NORTH ST	Housing complexes, including apartments	22229	0	0	0	0	0	375	22604	2	Long-Term
E SWAYZE ST	Housing complexes, including apartments	49533	24000	4000	0	2000	0	750	80283	2	Long-Term
E LINCOLN ST	Housing complexes, including apartments	35481	0	0	0	0	0	1000	36481	2	Long-Term
E LAFRENTZ LN		3876	12000	0	0	0	0	250	16126	2	Long-Term
MARKET ST & WEST RAMP		1695	0	0	0	0	0	0	1695	2	Long-Term
E STATE ST & N 1ST AVE		0	0	0	0	0	0	125	125	2	Long-Term
MARKET ST		0	0	0	0	0	0	125	125	2	Long-Term
EAST RAMP		0	0	0	0	0	0	375	375	2	Long-Term
3RD AVE & E LINN ST		0	0	0	0	0	0	125	125	2	Long-Term
WEST RAMP		0	0	0	0	0	0	125	125	2	Long-Term
RIVERSIDE ST		9857	0	0	0	0	0	3125	12982	2	Long-Term
E MADISON ST		0	0	0	0	0	0	375	375	2	Long-Term
S 14TH AVE		35019	0	0	0	0	0	0	35019	2	Long-Term
STRATFORD LN		0	0	0	0	0	0	375	375	3	Long-Term
W NORTH ST		29731	0	0	0	0	0	875	30606	3	Long-Term
E ANSON ST & S 7TH AVE		299	0	0	0	0	0	0	299	3	Long-Term
N 8TH ST		79513	28000	0	0	0	0	2125	109638	3	Long-Term
N 19TH ST		29314	0	0	0	0	0	1250	30564	3	Long-Term
JACKSON ST & S 9TH AVE		471	0	0	0	0	0	0	471	3	Long-Term
JACKIE TERR		0	0	0	0	0	0	250	250	3	Long-Term
CARSON DR		0	0	0	0	1000	0	875	1875	3	Long-Term

Table 3-A.2. City of Marshalltown Program Review - Implementation Plan

Location	Land Use	Sidewalk Remediation Cost	Pedestrian Ramp Remediation Cost	Bus Stops Remediation Cost	Traffic Signal Remediation Cost*	Crosswalk Remediation Cost	Railroad Crossing Remediation Cost	Sidewalk/Trail Barriers Remediation Cost	Total Cost	Priority Level	Term
WAKEFIELD DR		20149	16000	0	0	0	0	500	36649	3	Long-Term
N 10TH AVE		50590	0	0	0	0	0	1625	52215	3	Long-Term
RICHARD LN		43976	0	0	0	0	0	625	44601	3	Long-Term
MAPLE BLVD		0	0	0	0	0	0	125	125	3	Long-Term
MARSHALL DR		28070	0	0	0	0	0	1500	29570	3	Long-Term
KNOLLWAY DR		36025	0	0	0	0	0	1125	37150	3	Long-Term
E UNION ST		40418	0	0	0	0	0	375	40793	3	Long-Term
GRANDVIEW DR		0	12000	0	0	0	0	375	12375	3	Long-Term
FREIBERG CT & WESTWOOD DR		278	0	0	0	0	0	0	278	3	Long-Term
S 29TH ST		0	0	0	0	0	0	1125	1125	3	Long-Term
N 18TH ST		9631	0	0	0	0	0	1875	11506	3	Long-Term
ROLLING MEADOWS RD		5535	8000	0	0	0	0	625	14160	3	Long-Term
N 11TH AVE		16517	0	0	0	0	0	1875	18392	3	Long-Term
S 17TH AVE		9768	0	0	0	0	0	0	9768	3	Long-Term
S 11TH AVE		0	8000	0	0	500	0	625	9125	3	Long-Term
PLAYER ST & S 6TH ST		0	0	0	0	0	0	250	250	3	Long-Term
THOMAS DR		26553	0	0	0	0	0	250	26803	3	Long-Term
BENJAMIN DR		29218	20000	0	0	500	0	625	50343	3	Long-Term
N 17TH ST		17750	0	0	0	0	0	375	18125	3	Long-Term
EMERALD DR & S 14TH ST		407	0	0	0	0	0	0	407	3	Long-Term
E LINCOLN ST & N 2ND AVE		378	0	0	0	0	0	0	378	3	Long-Term
BLOSSOM LN		14021	0	0	0	0	0	375	14396	3	Long-Term
HIGHLAND ACRES RD		0	0	0	0	0	0	750	750	3	Long-Term
N 1ST ST & W NORTH ST		969	0	0	0	0	0	0	969	3	Long-Term
S 10TH AVE		29873	16000	0	0	1000	0	0	46873	3	Long-Term
WAKEFIELD CIR		11778	0	0	0	0	0	0	11778	3	Long-Term
S 16TH AVE		33707	0	0	0	0	0	375	34082	3	Long-Term
NORRIS PL		45630	0	0	0	0	0	1500	47130	3	Long-Term
WALTERS CIR		0	0	0	0	0	0	625	625	3	Long-Term
S 12TH AVE PL		23418	0	0	0	0	0	0	23418	3	Long-Term
S 15TH AVE		14428	0	0	0	0	0	0	14428	3	Long-Term
INNES BLVD		8660	0	0	0	0	0	375	9035	3	Long-Term
E ANSON ST & S 9TH AVE		431	0	0	0	0	0	0	431	3	Long-Term
GREENFIELD DR		17876	0	0	0	500	0	1375	19751	3	Long-Term
MAY ST & S 9TH AVE		1103	0	0	0	0	0	0	1103	3	Long-Term
RIVER OAKS APARTMENTS		0	48000	0	0	500	0	375	48875	3	Long-Term
WAUCONDA RD		0	0	0	0	0	0	375	375	3	Long-Term
COOPER LN		13010	0	0	0	0	0	375	13385	3	Long-Term
E MADISON ST & S 2ND AVE		1150	0	0	0	0	0	0	1150	3	Long-Term
PRAIRIE LN		10183	0	0	0	0	0	875	11058	3	Long-Term
ANSON ST		0	0	0	0	0	0	125	125	3	Long-Term
LILY LN		48747	8000	0	0	500	0	1250	58497	3	Long-Term
KESTER RD		6724	0	0	0	0	0	1625	8349	3	Long-Term
ARNOLD DR		0	0	0	0	0	0	2875	2875	3	Long-Term
BYRON ST		9587	0	0	0	0	0	0	9587	3	Long-Term
LELAND CT		0	0	0	0	0	0	500	500	3	Long-Term
JACKSON ST		39414	0	0	0	0	0	2625	42039	3	Long-Term
N 18TH AVE		0	8000	0	0	0	0	0	8000	3	Long-Term
Long-Term (2040 and Later) Totals		\$3,878,862	\$1,400,000	\$16,000	\$7,500	\$67,000	\$0	\$183,625	\$5,552,987	-	Long -Term
Marshalltown ADA Facilities Totals		\$7,420,802	\$3,060,000	\$68,000	\$45,000	\$133,000	\$5,000	\$350,375	\$11,131,748	-	-

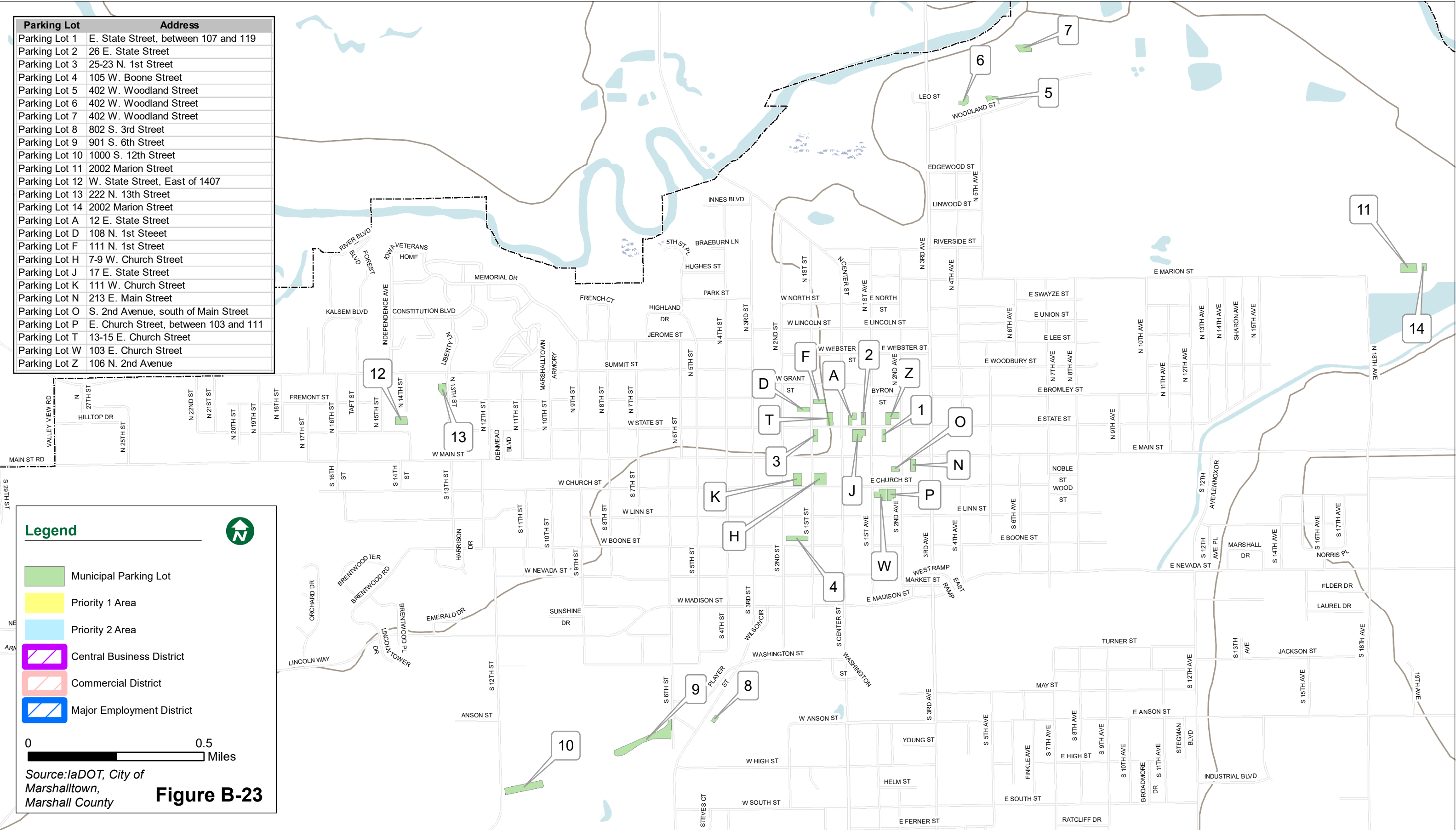


Table B-2. City of Marshalltown ADA Transition Plan and Inventory - City Parking Lot Review Matrix																				
	Lot Access	Number of Spaces and Dimensions						Access Aisle				Signage			Pavement Condition***	Sidewalk		Pedestrian Ramp		Priority
Parking Lot ID*	Access to PAR	Total Number of Spaces**	Total Number of Accessible Spaces	Meets Minimum Required # of Accessible Spaces per total spaces	Accessible Car Spaces are 8 Feet Wide or Greater	Van Accessible Spaces are 11 Feet or Greater (or 8 Feet or Greater if Access Aisle is 8 Feet or Greater)	1 or More Van Accessible Space per 6 Accessible Spaces	Access Aisle (Loading Area) Present for Each Space	Access Aisle Marked	Access Aisle Extends Full Length of Space Served	Access Aisle 5 Feet Wide or Greater	Accessible Parking Signs with International Accessibility Symbol Present	Van Accessible Sign Present	Bottom of Sign 5 Feet or Greater from Ground	Condition Rating of 1-4	Length Non-Compliant Sidewalk (Not associated with Street ROW)	Cost for Remediation	Number of Non-Compliant Ramps	Cost for Remediation	Priority Ranking
12	Yes	18	0	No	N/A	N/A	N/A	Yes	N/A	N/A	N/A	N/A	N/A	N/A	2	155	\$ 3,879.00	1	\$ 4,000.00	1
13	Yes	32	1	No	Yes	Yes	Yes	Yes	No	N/A	No	No	No	N/A	1					1
D	Yes	38	1	No	Yes	Yes	No	Yes	N/A	Yes	No	No	No	N/A	3					1
3	Yes	19	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	Yes	Yes	Yes	1					1
P	Yes	58	1	No	Yes	Yes	No	No	Yes	Yes	No	Yes	No	Yes	2	120	\$ 3,012.00			1
W	Yes	59	1	No	N/A	Yes	No	N/A	Yes	Yes	No	Yes	No	Yes	3					1
2	Yes	31	6	Yes	Yes	Yes	No	N/A	N/A	N/A	No	No	No	N/A	3					1
A	Yes	38	2	Yes	Yes	No	Yes	N/A	Yes	Yes	Yes	Yes	Yes	Yes	3					1
4	Yes	62	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	1					1
5	Yes	46	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	2			1	\$ 4,000.00	1
6	Yes	38	2	Yes	Yes	No	Yes	No	Yes	Yes	Yes	Yes	Yes	No	1			3	\$ 12,000.00	1
7	Yes	71	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	2			5	\$ 20,000.00	1
8	Yes	19	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	1			3	\$ 12,000.00	1
9	Yes	288	2	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	4	20	\$ 489.00	4	\$ 16,000.00	1
10	No	209	3	No	Yes	No	No	Yes	N/A	N/A	No	No	No	N/A	2					1
11	Yes	N/A	0	No	N/A	N/A	N/A	Yes	N/A	N/A	N/A	N/A	N/A	N/A	1			3	\$ 12,000.00	1
14	Yes	N/A	N/A	Yes	N/A	N/A	N/A	Yes	N/A	N/A	N/A	N/A	N/A	N/A	1					1
1	Yes	18	2	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	2					2
F	Yes	35	1	No	N/A	Yes	No	Yes	Yes	Yes	No	Yes	No	Yes	2					2
H	Yes	102	2	No	Yes	No	No	Yes	Yes	Yes	No	Yes	No	Yes	2					2
J	Yes	63	3	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	1			1	\$ 4,000.00	2
K	Yes	56	0	No	N/A	N/A	N/A	Yes	N/A	N/A	N/A	N/A	N/A	N/A	1					2
N	Yes	43	1	No	Yes	Yes	No	Yes	N/A	Yes	No	Yes	No	Yes	3	5	\$ 125.00	1	\$ 4,000.00	2
O	Yes	28	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	1					2
T	Yes	77	1	No	Yes	Yes	No	Yes	Yes	Yes	No	No	No	N/A	2					2
Z	Yes	49	1	No	N/A	Yes	Yes	N/A	Yes	Yes	Yes	Yes	No	Yes	2					2

*Parking Lot ID's correspond with Figure 23

**A value of "N/A" in this column indicates a parking lot has no parking stall striping. These lots were considered as not having the appropriate number of accessible spaces

*** Condition Ratings are defined as follows:

Level of Accessibility	Condition Rating	Recommended Action
Generally Accessible	1: Sidewalk is smooth with no vertical discontinuities	Maintain
	2: Sidewalk has vertical discontinuities less than a 1/2 inch, and the surface is still passable	Monitor
Generally Not Accessible	3: Sidewalk has vertical discontinuities more than 1/2 inch	Reconstruct or Perform Spot Improvements to Remove Barriers
	4: Sidewalk is crumbling, has many cracks, and is unpassable for wheelchairs in many spots	Reconstruct or Perform Spot Improvements to Remove Barriers

Appendix C: ADA Transition Plan Projects referenced from the City of Marshalltown Capital Improvements Plan

I. Appendix C: City of Marshalltown Capital Improvements Plan (CIP) Projects that Include (Consider) Accessible Pedestrian Infrastructure

1. Capital Improvements Plan

The City of Marshalltown's Capital Improvements Plan (CIP) ensures repairs, replacement, or expansion of public facilities, streets, parks, etc. are effectively planned and budgeted for by the City.

2. State Transportation Improvement Program

There is one project for the City of Marshalltown scheduled in the State Transportation Improvement Program (STIP) which is listed below.

CIP Area	Proposed Improvements	Year
Engineering	Parking Lot Renovation – 1 st /Church Street: \$865,000	2019-2020
Engineering	Railroad Crossing Safety Improvements	2020
Public Works	State Street Culvert Overlay	2022
Public Works	Annual Street Resurfacing: \$100,000 (2019); \$350,000 (2020-2023)	2019-2023
Public Works	Concrete Repairs: \$100,000 (2019); \$200,000 (2020-2023)	2019-2023
Public Works	Annual ADA Sidewalk Construction: \$50,000 Annually	2019-2023
Public Works	12 th Avenue Sidewalk Installation	2019-2020
Street	Mill and Overlay of Streets with Tornado Impact	2021

The City will continue to implement infrastructure and maintenance improvements as part of its long-range community investment planning and pavement management plan. With these efforts, the City will incorporate ADA-compliant pedestrian facilities and complete accessible infrastructure improvements as needed to ensure access for all users.

Appendix D: Public Outreach Results

I. Appendix D: Public Outreach Results

Input from the community has been gathered and used to help define priority areas for improvements within the City of Marshalltown public rights-of-way.

Public outreach for the creation of this document consisted of the following activities:

- **Public Open Houses:**

The City of Marshalltown held a public open house on August 22, 2019 at the Marshalltown Public Library to gather input on the state of pedestrian infrastructure in the City and priority preferences for projects moving forward. Comments received during the open house are documented in **Figure D-1** below.

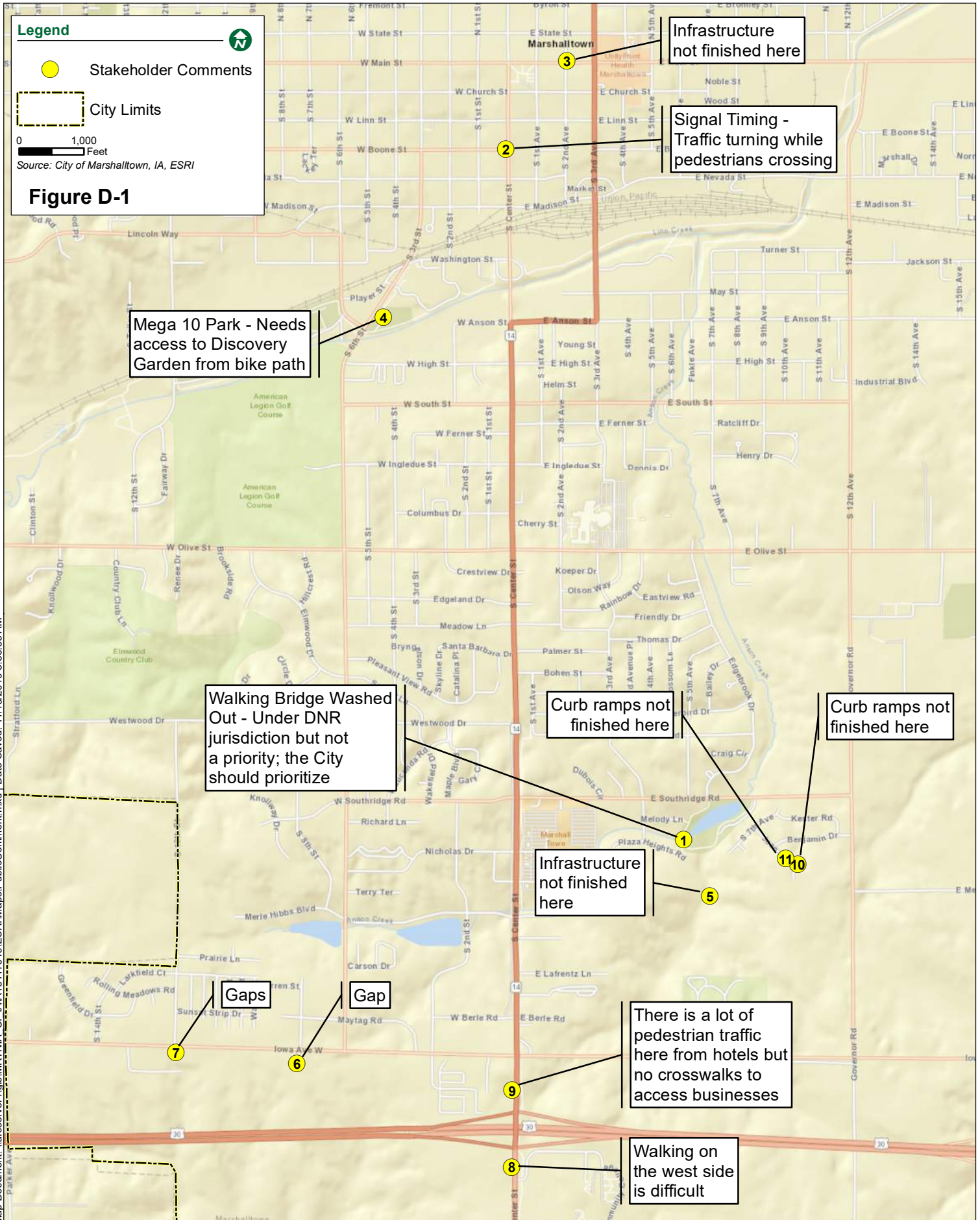
- **Project Website:**

The project website was live for the duration of the project and provided notifications, contact information, project updates and information, and an interactive grievance procedure in which participants can report on areas in the community that are not accessible. This document was also available for public comment on the website between the dates of January 20, 2020 and February 17, 2020.

- **Outreach to Local Stakeholder Groups:**

Various community groups were contacted early in the process that include or represent those with disabilities in the Marshalltown community. Project staff reached out to the following groups during this process:

- | | |
|--------------------------------|---|
| • Ames Rentals | • Superior Rentals |
| • Associates Supported Housing | • Trilogy/Embers of Marshalltown |
| • Compassionate Properties | • Boulder Properties |
| • Grant Park LLC | • Southern Hills Cooperative |
| • Historic Tallcorn Towers | • Mid Iowa Workshop |
| • ILQ, Inc. | • CIRSI (Central Iowa Residential Services) |
| • Iowa Odd Fellow Housing Corp | • Senior Citizens Center |
| • K & K Preferred Properties | • Northeast Iowa Area Agency on Aging |
| • Kading Properties LLC | • Region 6/PeopleRides |
| • The Willows of Marshalltown | • Accura Healthcare of Marshalltown (fka Hawkeye Care Center) |
| • VFW Post 839 | • Bickford of Marshalltown |
| • L & K Rentals | • Embers of Marshalltown |
| • Mtown/Westown Apts | • Glenwood Place Retirement Community |
| • Oetker, Deb | • Grandview Heights |
| • River Birch | • Iowa Veterans Home |
| • River Oaks | • Southridge Specialty Care |
| • Sundance Apts | • Sundance Apts |



Appendix E: Grievance Procedure

I. Appendix E: City of Marshalltown ADA Grievance Procedure

In accordance with 28 CFR 35.107(b), the City has developed the following ADA grievance procedure for the purpose of the prompt and equitable resolution of citizens' complaints, concerns, comments, and other grievances.

The City understands that members of the public may desire to contact staff to discuss ADA issues without filing a formal grievance. Members of the public wishing to contact the ADA Coordinator should reference the contact information in **Appendix F**. Contacting staff to informally discuss ADA issues is welcome and does not limit a person's ability or right to file a formal grievance later.

Those wishing to file a formal written grievance with the City of Marshalltown may do so by one of the following methods:

A. Telephone

Contact the City of Marshalltown's ADA Coordinator listed in the **Contact Information** section of **Appendix F** to submit an oral grievance. The staff person will use the internet to electronically submit the grievance on behalf of the person filing it.

B. Paper Submission

A paper copy of the City's grievance form is available by request from the City of Marshalltown's ADA Coordinator (contact information in **Appendix F**). Complete the form and submit it to the City of Marshalltown ADA Coordinator at the address listed.

The City of will acknowledge receipt of the grievance to the citizen within 10 working days of the submission. City staff will then provide a response or resolution to the grievance or will provide information on when the citizen can expect a response. If the grievance filed does not fall within the City of Marshalltown's jurisdiction, staff will work with the citizen to contact the agency with jurisdiction.

When possible (typically within 60 calendar days or less of the grievance submission) City staff will conduct an investigation to determine the validity of the alleged violation. As a part of the investigation, internal staff will be consulted to fully understand the complaint and possible solutions. The City of Marshalltown staff will contact the citizen to discuss the investigation and proposed resolution.

The City of Marshalltown will consider all grievances within its particular context or setting. Furthermore, the department will consider many varying circumstances including: access to applicable services, programs, or facilities; the nature of the disability; essential eligibility requirements for participation; health and safety of others; and degree to which a potential solution would constitute a fundamental alteration to the program, service, or facility, or cause undue hardship to the City of Marshalltown.

Accordingly, the resolution by The City of Marshalltown of any one grievance does not constitute a precedent upon which the City is bound or upon which other complaining parties may rely.

Complaints of Title II violations may be filed with the Department of Justice (DOJ) within 180 days of the date of discrimination. In certain situations, cases may be referred to a mediation program sponsored by the DOJ. The DOJ may bring a lawsuit where it has investigated a matter and has been unable to resolve violations.

For more information, contact:

U.S. Department of Justice

Civil Rights Division

950 Pennsylvania Avenue, N.W.

Disability Rights Section – NYAV

Washington, D.C. 20530

www.ada.gov

(800) 514-0301 (voice)

(800) 514-0383 (TTY)

Title II may also be enforced through private lawsuits in Federal court. It is not necessary to file a complaint with the DOJ or any other Federal agency, or to receive a "right-to-sue" letter, before going to court.

File Retention

The City of Marshalltown shall maintain ADA grievance files on behalf of the City for a period of seven years.

City Marshalltown, IA ADA Grievance Form

Instructions: Please fill out this form completely and submit to:

City of Marshalltown ADA Coordinator
City of Marshalltown, IA
24 N Center Street,
Marshalltown, IA 50158

Or it can be e-mailed to: jnickel@marshalltown-ia.gov

Complainant – person filing grievance:

Name: _____ Date _____

Address: _____ City, State, Zip Code: _____

Home: _____ Cell: _____

Work: _____ Email: _____

Representing – person claiming an accessibility issue or alleging an ADA violation (if not the complainant):

Name: _____

Address: _____ City, State, Zip Code: _____

Home: _____ Cell: _____

Work: _____ Email: _____

Description and location of the alleged violation and the nature of a remedy sought.

If the complainant has filed the same complaint or grievance with the United States Department of Justice (DOJ), another federal or state civil rights agency, a court, or others, the **name of the agency or court where the complainant filed it and the filing date**.

Agency or Court: _____ Contact Person: _____

Address: _____ City, State, Zip Code: _____

Phone Number: _____ Date Filed: _____

Appendix F: Contact Information

I. Appendix F: Responsible Officials and Key Staff

A. Responsible Officials Contact Information

1. City of Marshalltown ADA Coordinator:

Jessica Kinser

City of Marshalltown, City Administrator

24 N Center Street

City of Marshalltown, IA 50158

641-754-5799

jkinser@marshalltown-ia.gov

Appendix G: ADA Design Standards and Procedures

I. Appendix G: ADA Design Standards and Procedures

A. Design Standards

1. Public Rights-of-Way Accessibility Guidelines

Public Rights-of-Way Accessibility Guidelines (PROWAG), developed by the Access Board, are draft guidelines that address accessibility in the public rights-of-way. Sidewalks, street crossings, and other elements of the public rights-of-way present unique challenges to accessibility for which specific guidance is considered essential. The Access Board is developing these guidelines that will address various issues, including access for visually impaired pedestrians at street crossings, wheelchair access to on-street parking, and various constraints posed by space limitations, roadway design practices, slope, and terrain. PROWAG can be found at <http://www.access-board.gov/attachments/article/743/nprm.pdf>.

2. Proposed Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way

The Access Board is proposing these accessibility guidelines for the design, construction, and alteration of pedestrian facilities in the public right-of-way. The guidelines ensure that sidewalks, pedestrian street crossings, pedestrian signals, and other facilities for pedestrian circulation and use constructed or altered in the public right-of-way by state and local governments are readily accessible for pedestrians with disabilities. When the guidelines are adopted, with or without additions and modifications, as accessibility standards in regulations issued by other federal agencies implementing the ADA, Section 504 of the Rehabilitation Act, and the Architectural Barriers Act, compliance with these accessibility standards is mandatory. These proposed accessibility guidelines can be found on the Access Board website (<http://www.access-board.gov>) under Public Rights-of-Way or at <http://www.access-board.gov/attachments/article/743/nprm.pdf>.

3. Accessible Public Rights-of-Way Planning and Design for Alterations (August 2007)

This report and its recommendations are the work of the Public Rights-of-Way Access Advisory Committee (PROWAAC) – Subcommittee on Technical Assistance and are intended to provide technical assistance only. The report is not a rule and has no legal effect. It has not been endorsed by the U.S. Access Board, the Department of Justice, or the Federal Highway Administration of the Department of Transportation. Still it can be a technical advisory source for engineers and technicians who are planning and designing for alterations to pedestrian elements. This document is on the Access Board website (<http://www.access-board.gov>) or at <http://www.access-board.gov/attachments/article/756/guide.pdf>.

4. Iowa Department of Transportation

Title II of the ADA requires:

- New facilities be designed and constructed such that the facility is “readily accessible to and usable by individuals with disabilities.”
- Existing facilities be altered such that the altered portion of the facility “is readily accessible to and usable by individuals with disabilities.”

ADA accommodation is considered throughout the development process beginning with the project concept and continuing through final design. This includes working with local officials regarding design considerations and cost sharing when applicable.

In order to address ADA accommodation early in the process, the project concept memorandum includes a section/paragraph on ADA accommodation requirements for the project. The project concept memorandum develops alternatives and recommends a final alternative to carry forward into preliminary design. Project development begins with the completion of the project concept memorandum issued by the Office of Design, Office of Location and Environment, or one of the six district offices.

The following provides guidance to:

- Ensure new pedestrian facilities are designed and constructed to be ADA compliant,
- Bring existing pedestrian facilities into compliance with ADA, and
- Address ADA compliance for pedestrian facilities during construction.

New Pedestrian Facilities

To assist both designers and contractors, the Iowa DOT will provide guidance for designing and constructing ADA-compliant pedestrian facilities.

Guidance for Designers

The Iowa DOT has published specific instructions in Chapter 12 of the Design Manual, which is based on PROWAG – to assist designers with designing ADA-compliant facilities. The guidance is updated as new information from the U.S. Access Board, U.S. Department of Justice, and Federal Highway Administration (FHWA) is released.

Guidance for Contractors

The Iowa DOT will provide contractors with detailed plan sheets for laying out curb ramps and landings to assist contractors with building ADA-compliant facilities. In addition, the Iowa DOT will modify the Standard Specifications when necessary to comply with changes to design standards.

Existing Pedestrian Facilities

The guidance provided in Chapter 12 of the Design Manual also applies to the improvement of existing pedestrian facilities. Existing pedestrian facilities will be brought to ADA compliance by:

- Verifying that features such as sidewalk widths, slopes, surfaces, and changes in level do not violate guidelines,

- Installing or replacing out-of-compliance features such as curb ramps and landings, and
- Installing or replacing detectable warnings.

ADA Accommodation during Construction

When pedestrian facilities are disrupted during construction, detour routes and open walkways (or sidewalks) should provide accessibility in accordance with Design Manual Chapter 12-A4(e) Temporary Pedestrian Facilities.

Additional steps to consider include:

- Detour pedestrians to the next block to avoid the construction area. This could include a change order to use flaggers to keep pedestrians on the detour.
- Place closures at the sidewalks and notify advocacy agencies of the closures as required by Section 2528.01;A910) of the Standard Specifications.
- Install temporary sidewalk.
- Install traffic control devices to channel pedestrians through the construction zone when a detour is not needed.
- Stage the work to minimize the impact to pedestrians and accelerate sidewalk construction.

Chapter 12 of the Iowa DOT Design Manual can be found at <https://iowadot.gov/design/design-manual>.

5. The Institute for Transportation at Iowa State University - Iowa Statewide Urban Design and Specifications (SUDAS)

The Iowa SUDAS is made up of two parts, a design manual and a specifications manual. The Design Manual is a document written to the engineer developing a specific project. It provides design guidance through references to appropriate national standards and regulations in Iowa. The Design Manual is a direct complement to the SUDAS Standard Specifications. It has been developed and updated with the involvement of engineers from cities, counties, state agencies, and consultants from across the state.

The Specifications Manual is written to the contractor providing specific requirements through the specifications for the contractor to complete the project. These standard specifications were developed with the involvement of engineers and construction industry representatives from across the state. The initial division includes the specific bidding requirements, legal requirements related to public contracts in Iowa, and contractual requirements that must be met to successfully complete the project.

Standard specification details for pedestrian facilities are include in **Appendix G-1** below. The Iowa SUDAS can be found at: <https://iowasudas.org/>

B. Design Procedures

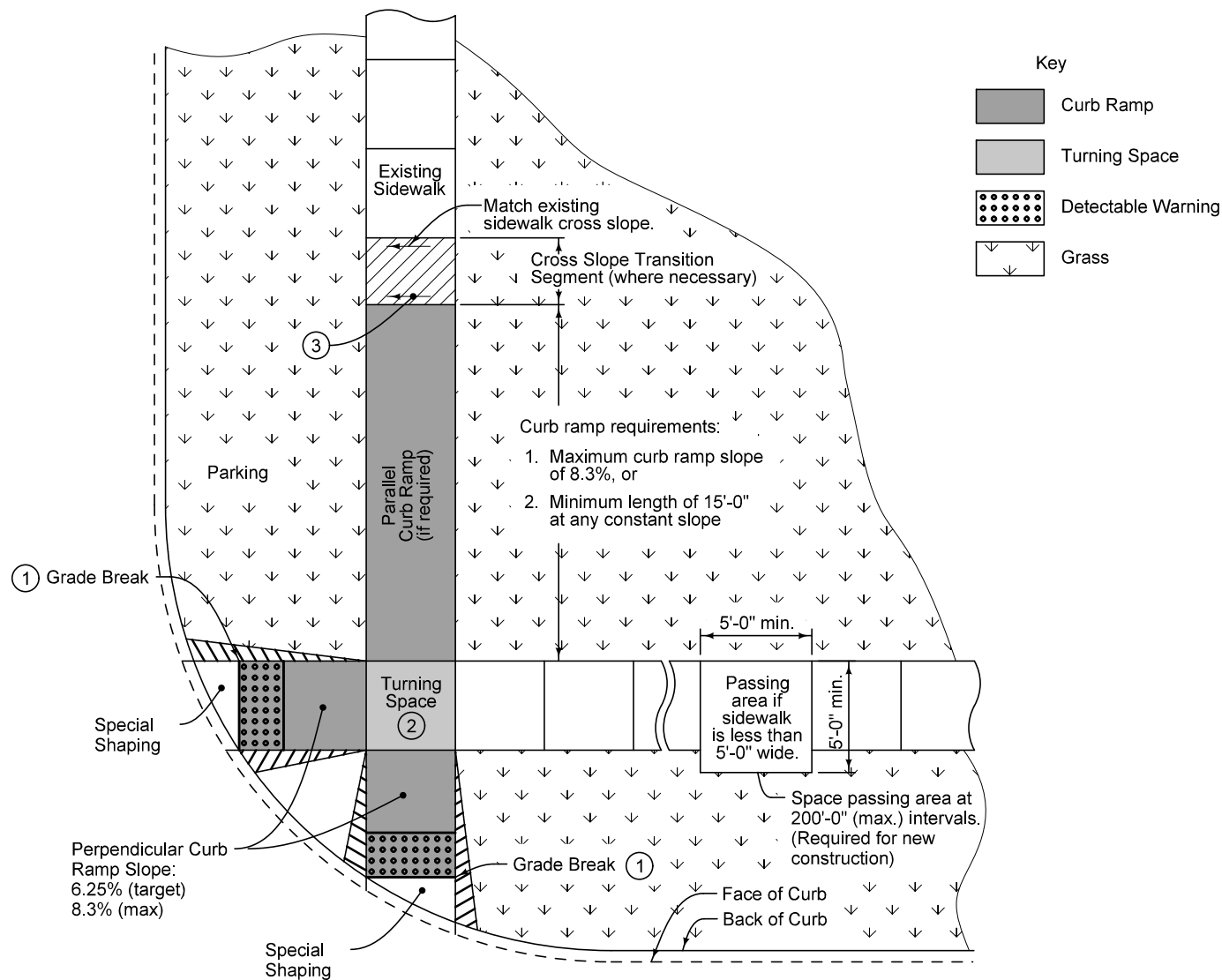
1. Intersection Corners

The City of Marshalltown will attempt to construct or upgrade pedestrian curb ramps to be consistent with Access Board recommended designs parameters and Iowa DOT ADA design guidance and procedures to the extent feasible within all capital reconstruction projects. There may be limitations which make it technically infeasible for the City to achieve these goals at an intersection corner within the scope of any project. Those limitations will be noted, and the intersection corners will be constructed to maximize accessibility to the extent feasible within the project scope. As future projects or opportunities arise, additional improvements at these locations may be incorporated into future projects. The City will strive to ensure that each intersection corner is constructed to be as accessible as possible.


2. Sidewalks / Trails

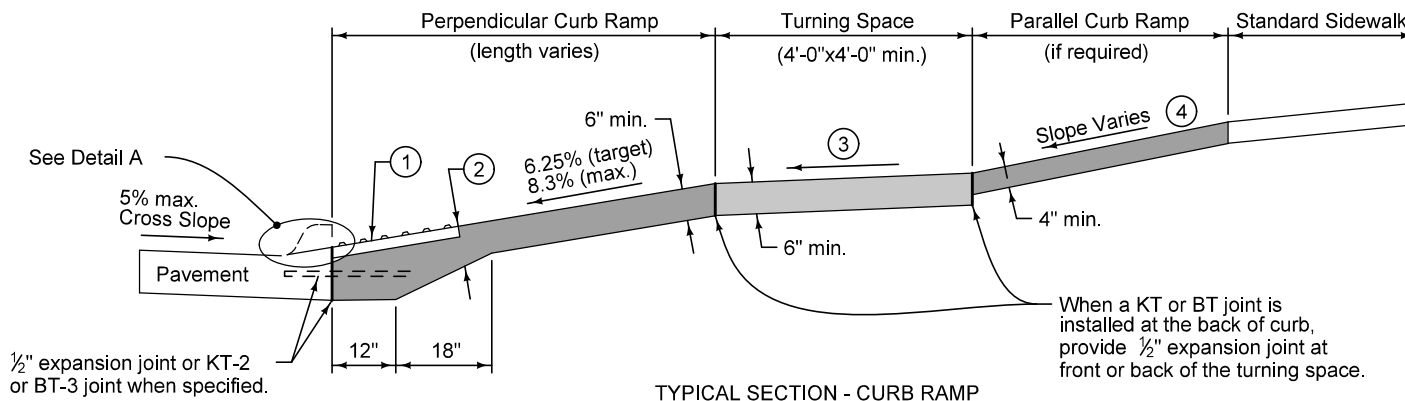
The City of Marshalltown will attempt to construct or upgrade sidewalks and trails to be consistent with Access Board recommended designs parameters, Iowa DOT ADA design guidance and procedures to the extent feasible within all capital improvement projects. There may be limitations which make it technically infeasible for the City to achieve these goals within all segments of sidewalks or trails within the scope of any project. Those limitations will be noted and those segments will be constructed to maximize accessibility to the extent feasible within the project scope. As future projects or opportunities arise, additional improvements at these locations may be incorporated into future projects. The City will strive to ensure that every sidewalk or trail is constructed to be as accessible as possible.

Appendix G-1. Iowa Statewide Urban Design and Specifications Pedestrian Facility Details






- 1 Match pedestrian street crossing slope, or flatter.
- 2 Minimum 4 feet by 4 feet. Target cross slope of 1.5% with a maximum cross slope of 2.0%.
- 3 Target cross slope of 1.5% with a maximum cross slope of 2.0%.

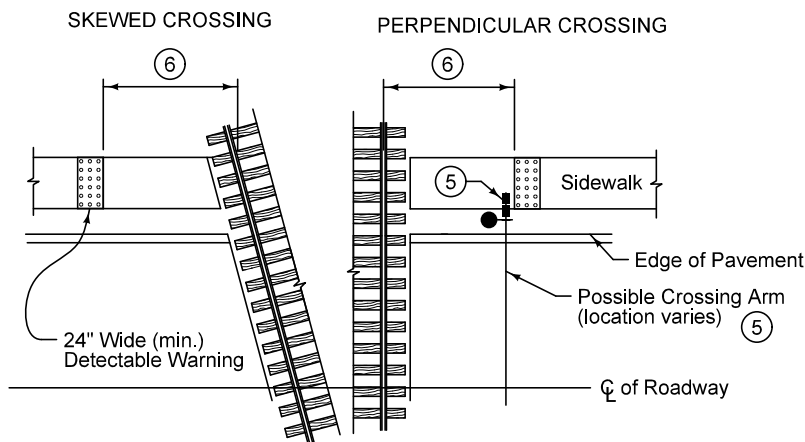
	REVISION	
	3	10-20-15
	7030.204 SHEET 1 of 1	
SUDAS Standard Specifications		
GENERAL FEATURES OF AN ACCESSIBLE SIDEWALK		



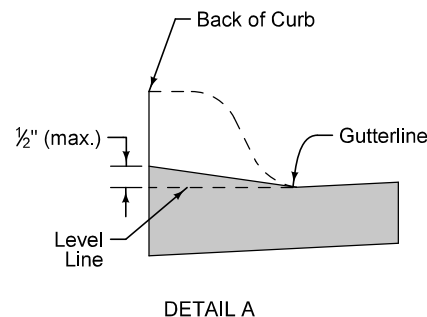
TYPICAL SECTION - CURB RAMP

Key

-  Curb Ramp
-  Turning Space
-  Detectable warning




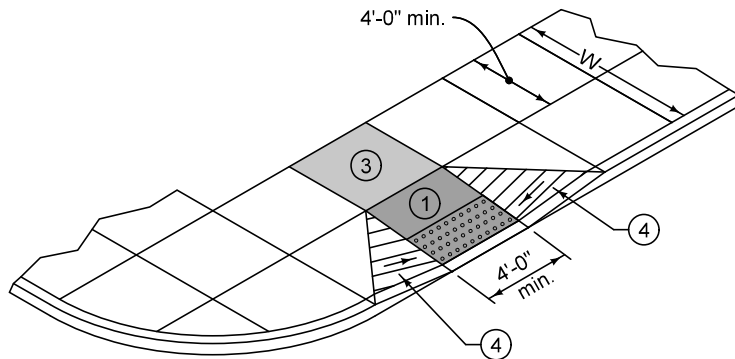
DETECTABLE WARNING LOCATION AT RAILROAD CROSSING



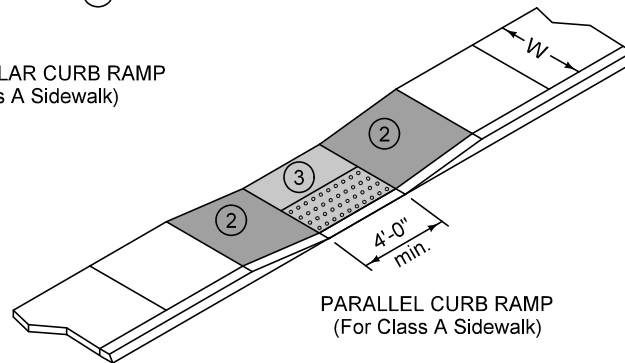
DETAIL A

- 1 Provide a minimum 2 foot width of detectable warning surfaces in the direction of pedestrian travel across the full width of the curb ramp or turning space, exclusive of curbs or flares.
 - 2 Provide a minimum of 6 inches of concrete below the detectable warning panel.
 - 3 Minimum 4 feet by 4 feet. Target cross slope of 1.5% with a maximum cross slope of 2.0%.
 - 4 If normal sidewalk elevation cannot be achieved with the perpendicular ramp between the street and landing due to limited ramp length, provide a parallel ramp to make up the elevation difference between the landing and the standard sidewalk.
- The length of the parallel ramp is not required to exceed 15 feet, regardless of the resulting slope. Do not exceed 8.3% slope for parallel ramps shorter than 15 feet.
- 5 If crossing gate conflicts with location of detectable warning or if pedestrian crossing gate is provided, place detectable warning panel in advance of the crossing gate.
 - 6 Locate front edge of detectable warning panel 12 to 15 feet from centerline of nearest rail. Orient truncated domes parallel to the direction of pedestrian travel.

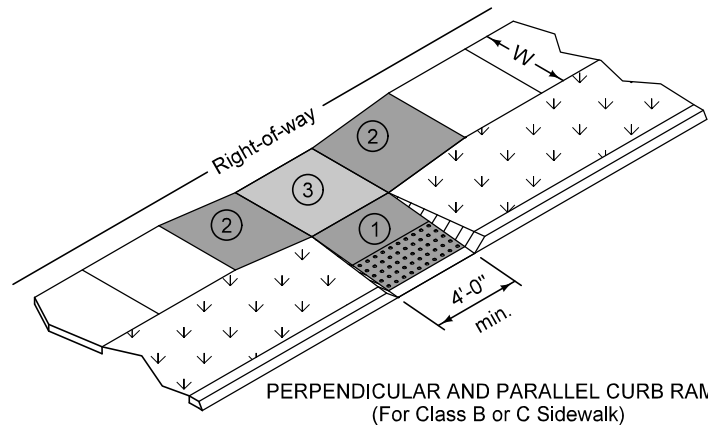
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GENERAL SIDEWALK AND CURB RAMP DETAILS		



PERPENDICULAR CURB RAMP
(For Class A Sidewalk)






PARALLEL CURB RAMP
(For Class A Sidewalk)

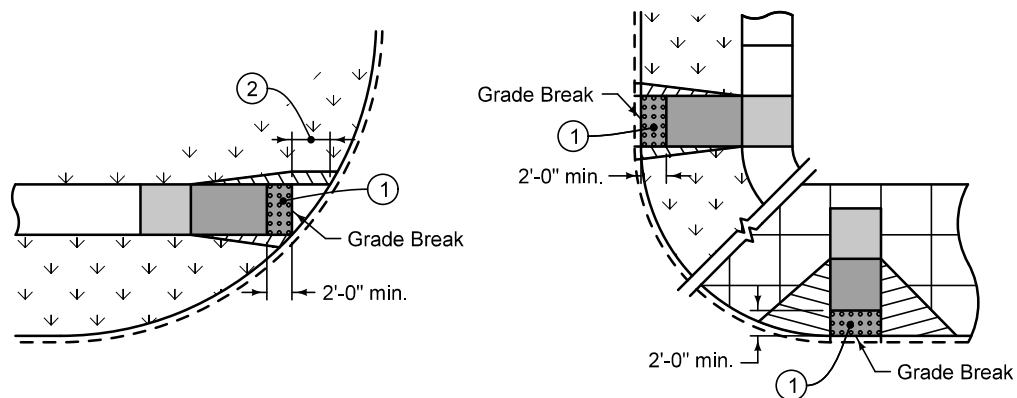


PERPENDICULAR AND PARALLEL CURB RAMP
(For Class B or C Sidewalk)

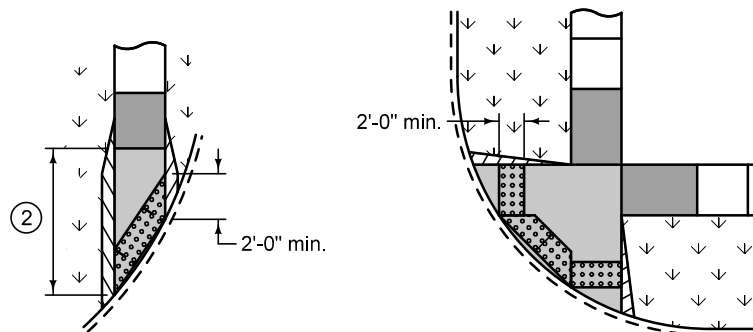
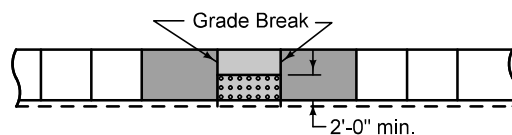
Key

-  Curb Ramp
-  Turning Space
-  Detectable Warning

- ① Perpendicular Curb Ramp: Target running slope of 6.25% with maximum running slope of 8.3%. Match pedestrian street crossing cross slope at back of curb. At mid-block crossings, cross slope may exceed 2.0% to match roadway grade.
- ② Parallel Curb Ramp: Target cross slope of 1.5% with a maximum cross slope of 2.0%. The length of the parallel ramp is not required to exceed 15 feet, regardless of resulting slope. Do not exceed 8.3% slope for parallel ramps shorter than 15 feet.
- ③ Turning Space: Target slope of 1.5%, with a maximum slope perpendicular to the travel directions of 2.0%. At mid-block crossings, cross slope of landing may exceed 2.0% to match roadway grade. Minimum 4 feet by 4 feet.
- ④ Flare (10:1 max.) required if ramp is contiguous with sidewalk.



PERPENDICULAR RAMPS



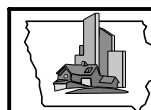
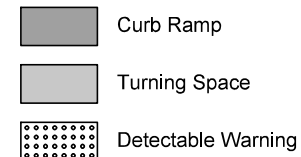
PARALLEL RAMPS

Provide a minimum 2 foot width of detectable warning surfaces in the direction of pedestrian travel across the full width of the curb ramp or turning space, exclusive of curbs or flares.

- ① When detectable warning is located on curb ramp surface, orient domes in the direction of pedestrian travel.
- ② When the distance between the grade break and the back of curb is less than 5 feet, place detectable warning surface at the bottom of the curb ramp.

Where one corner of the curb ramp is more than 5 feet from the back of curb, construct curb ramp as a parallel curb ramp. Move grade break back as required to place detectable warning on turning space at the back of curb.

Key

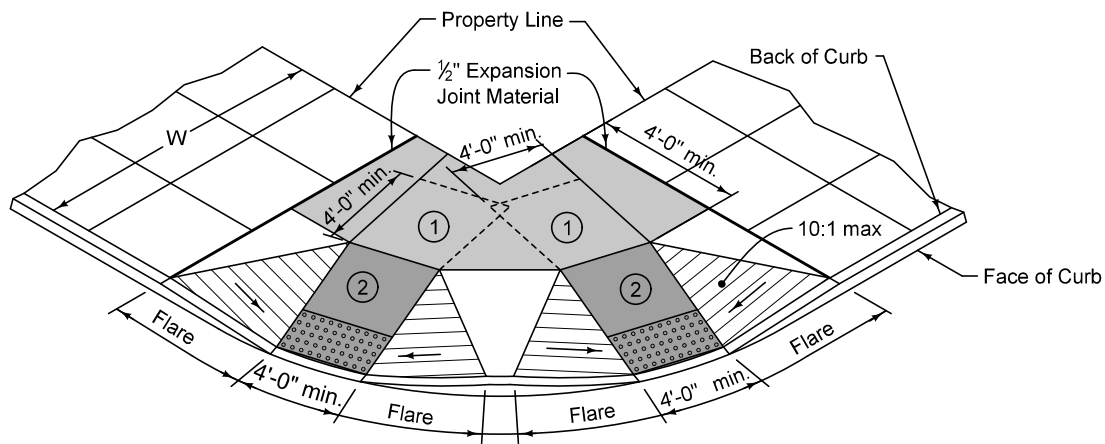


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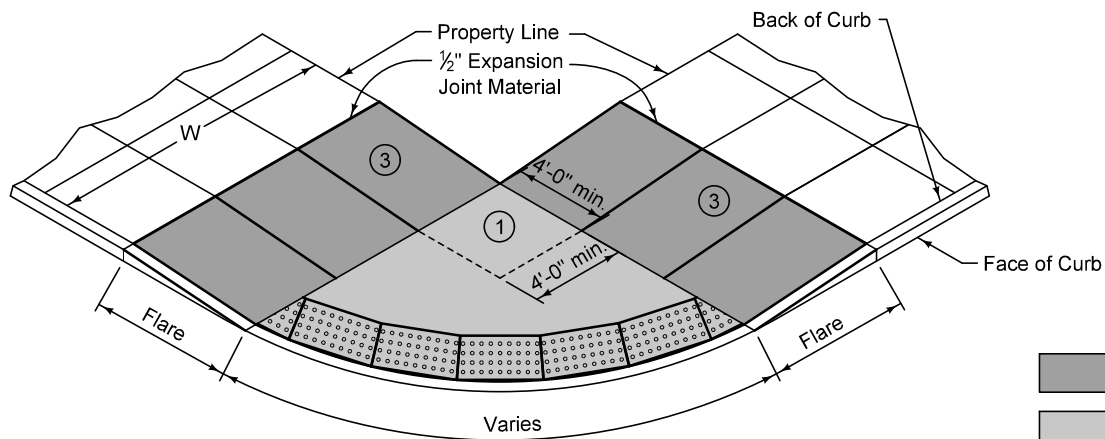
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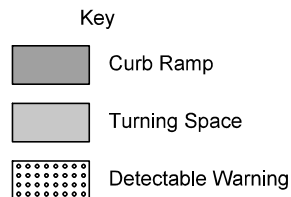
DETECTABLE WARNING
PLACEMENT




CLASS A SIDEWALK CURB RAMP

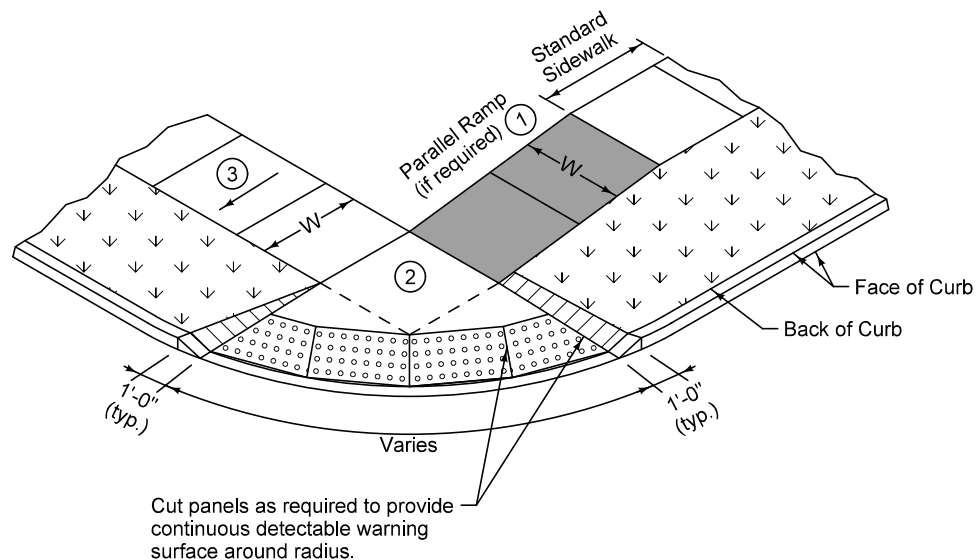


CLASS A SIDEWALK CURB RAMP ALTERNATIVE



- ① Turning Space: Target slope of 1.5% with maximum slope perpendicular to the travel direction of 2.0%. Minimum 4 feet by 4 feet (turning spaces may overlap).
- ② Perpendicular Curb Ramp: Target running slope of 6.25% with maximum running slope of 8.3%.
- ③ Parallel Curb Ramp: Target running slope of 6.25% with maximum running slope of 8.3%. The length of the parallel ramp is not required to exceed 15 feet, regardless of the resulting slope. Do not exceed 8.3% for parallel ramps shorter than 15 feet.

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CURB RAMPS FOR CLASS A SIDEWALK	



- ① Parallel Curb Ramp: If normal sidewalk elevation cannot be achieved with the perpendicular ramp between the street and landing due to limited ramp length, provide a parallel ramp to make up the elevation difference between the landing and the standard sidewalk.

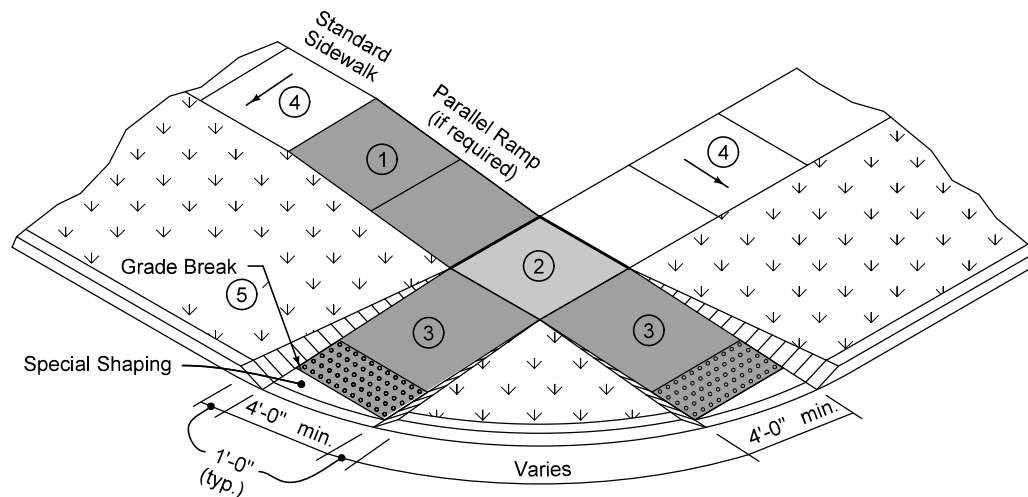
The length of the parallel ramp is not required to exceed 15 feet, regardless of the resulting slope. Do not exceed 8.3% slope for parallel ramps shorter than 15 feet.

- ② Turning Space: Target slope of 1.5% with maximum slope perpendicular to the direction of travel of 2.0%. Minimum 4 feet by 4 feet.
- ③ Target cross slope of 1.5% with a maximum cross slope of 2.0%.

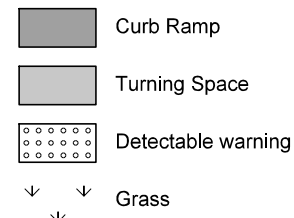
Key

	Curb Ramp
	Turning Space
	Detectable warning
	Grass

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ALTERNATIVE CURB RAMP FOR CLASS B OR C SIDEWALK	




Key



- ① Parallel Curb Ramp: If normal sidewalk elevation cannot be achieved with the perpendicular ramp between the street and landing due to limited ramp length, provide a parallel ramp to make up the elevation difference between the landing and the standard sidewalk.

The length of the parallel ramp is not required to exceed 15 feet, regardless of the resulting slope. Do not exceed 8.3% slope for parallel ramps shorter than 15 feet.

- ② Turning Space: Target slope of 1.5% with maximum slope perpendicular to the travel directions of 2.0%. Minimum 4 feet by 4 feet.
- ③ Perpendicular Curb Ramp: Target running slope of 6.25% with maximum running slope of 8.3%.
- ④ Target cross slope of 1.5% with a maximum cross slope of 2.0%.
- ⑤ Match pedestrian street crossing cross slope or flatter.

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CURB RAMP FOR CLASS B OR C SIDEWALK		

PART III – City of Marshalltown ADA Self-Evaluation & Transition Plan for Public Building Facilities

I. OVERVIEW

As part of the City of Marshalltown ADA Transition Plan, the City evaluated government buildings and other public buildings and facilities within City parks such as restroom facilities, park shelters, and community buildings. The following public facilities were inventoried:

A. City of Marshalltown Government Building Facilities

- City of Marshalltown Municipal Airport – 2651 170th Street
- City of Marshalltown City Hall and Carnegie Building – 24 N Center Street
- City of Marshalltown Public Works – 36 N Center Street
- City of Marshalltown Water Pollution Plant – 1001 Woodland Street

B. City of Marshalltown Public Park Building Facilities

- Anson Park – 301 E Anson Street
- Assistance League Park – 2013 Gethmann Drive
- Bicentennial Park – 507 Grandview Drive
- Kiwanis Park – 2206 S 3rd Avenue
- Marshalltown Softball Complex – 901 S 6th Street
- Mega 10 Park – 802 3rd Street
- Nicholson-Ford OVH Park – E Marion Street
- Riverview Park – 402 Woodland Street
- Timber Creek Park – 2223 Edgebrook Drive

The following pages include facility-specific ADA Self-Evaluation checklists for each facility.